

North Coast Line System North Information Pack

Rocklands to Cairns

Version Information

Version 3.0: 05/10/2016

- Removed Aurizon Network Sections
- Updated References Queensland Rail Network to Queensland Rail
- Updated References Queensland Transport to DTMR
- Removed references 2005 Access Undertaking
- EPA changed to Department
- Updated Standards references
- Updated Line Diagrams
- Updated Climate Information
- Updated Rail System Electrification
- Updated Track Grade
- Updated Network Control Regions & Signalling Centres
- Updated Safeworking Systems
- Update Noise Management System

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Introduction

The detail provided in this pack relates to infrastructure and operational information necessary to develop a conceptual operating plan/Access Application. It is envisaged that Access Seekers will liaise closely with Queensland Rail to formulate a detailed operating specification as part of a full access agreement negotiation. Operational parameters outlined in this pack may be varied by mutual agreement with **Queensland Rail**.

All railway operators, wishing to operate in Queensland, require Accreditation under the Transport Infrastructure Act 1994 (Qld) and need to consider, but not limited to, the following aspects of typical rail operations:-

- Provisioning, stabling or stowing areas for rollingstock
- Train crewing
- Safeworking
- Training
- Route knowledge
- Environmental requirements
- Track standards
- Signalling and traction systems standards and constraints
- Safety training
- Management of risk
- Rollingstock registration and Train authorisation
- Legal issues as contained in Queensland Rail's Access Undertaking, Access Agreements and information contained in this pack.

Operators will be required to have accreditation with the Department of Transport and Main Roads, hold an Access Agreement with **Queensland Rail** and meet any conditions and precedents specified in the Access Agreement prior to commencing operations.

Accreditation means an applicant has confirmed that they are able to meet the requirements to carry out railway operations in Queensland. The Director-General, the Department of Transport and Main Roads, must be satisfied that the applicant has demonstrated:

- Effective management and control of rolling stock
- Competence and capacity to manage risks to safety associated with railway operations
- Competence and capacity to implement the required safety management system and has met the legislative requirements
- Financial capacity, or public risk insurance arrangements for potential liabilities.

Contact details are:

Customer Services, Safety and Regulation Division:
Ph: 07 3066 2689
Email: rsr@tmr.qld.gov.au
Post: PO Box 673
Fortitude Valley QLD 4006
Web: www.tmr.qld.gov.au

Operators need to be aware of and comply with other general legislation such as but not limited to Workplace Health & Safety, Environmental legislation and Heritage legislation.

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This package is issued to railway operators as an UNCONTROLLED DOCUMENT and is reviewed annually. It is the onus of railway operators to ensure they are using the current version of this document.

This Information Pack is provided for information purposes only and Queensland Rail does not make any representation or warranty, express or implied, as to the accuracy, suitability or completeness of the information. To the extent that any inconsistency arises between this Information Pack and the Access Agreement or Queensland Rail's Access Undertaking, the provisions of the Access Agreement and Queensland Rail's Access Undertaking shall prevail.

General Information

The North Coast Line System is located the length of Coastal Queensland between Cairns (16°55' S - 145°46' E) in the north and Brisbane (27°27' S - 153°01' E) in the south. The line extends over approximately 1400km excluding sections of Aurizon network between Parana and Rocklands in central Queensland and Kaili and Durroburra in north Queensland.

Due to the length of this System there will be two information packs, **SOUTH** - Roma Street to Parana and **NORTH** - Rocklands to Cairns.

North



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The North Coast line (North) system carries the following:

- various freight products, including containerised and industrial freight, minerals, livestock and bulk commodities including sugar and grain
- containerised freight services operate between Brisbane and major centres in central and north Queensland, including Rockhampton, Mackay, Townsville and Cairns
- sugar traffic is hauled from sugar mills to the Ports of Mackay and Townsville
- minerals traffic from the Mount Isa Line runs on the NCL from Stuart to the Port of Townsville
- industrial products e.g. cement and fuel are hauled on the NCL from the Port of Townsville to Stuart before joining the Mount Isa Line to commence its journey to the mining centres of Cloncurry and Mount Isa
- acid is similarly hauled on the North Coast line from the Sun Metals Refinery to Stuart, before travelling on the Mount Isa Line to the Phosphate Hill fertiliser plant

Long distance passenger and high speed Tilt Train services also operate on the line servicing central and north Queensland.

The North Coast line (North) adjoins three other major rail systems, the Aurizon Blackwater System between Parana and Rocklands (in Central Queensland) and the Aurizon Newlands System between Kaili and Durroburra (in north Queensland).

Descriptive distances within this document (unless otherwise stated) are based on physical kilometre posts in the field and are to be used only as location descriptors ie they do not compensate for equalities resulting from deviations. Access charges and performance statistics are generated using actual through distances derived from relevant Working Plan and Sections and reflected on Line Code Diagrams. Generally distances originate from the junction of the branch and commence at 0 km.

General Climate - Queensland Wide

The system is generally in a warm to hot temperature climate.

The following sub-sections specify general climatic parameters. For latest and more specific information potential railway operators should consult The Australian Bureau of Meteorology at its Internet Website: <http://www.bom.gov.au/climate>

Cyclones

Tropical lows, which develop from November to April, occasionally deepen to cause tropical cyclones. Tropical cyclones show great variation in behaviour. They foster high winds, heavy, flood-producing rainfall (especially when a cyclone moves over high ground), and coastal storm surges.

The high wind risk does not usually extend further inland than 50 km. Inland movement reduces the inflow of moisture and cyclone intensity declines, often within a few hours.

Not all cyclones are severe.

Cyclones may impact on this System. Cyclones have the capability of affecting the entire North Coast Line due to “knock on” affects.

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Humidity

This region could experience prolonged periods of high humidity and potential railway operators should consider this when planning / designing rollingstock and machinery to operate on this rail system.

Rainfall

The wettest places in Queensland are located on the tropical coast between Innisfail and Cairns.

Highest rainfall occurs on the seaward side of the Great Divide.

However, at times in summer the inland extension of low-level moist airflow, in combination with intense surface heating, produces significant thunderstorm activity. Rainfall is mostly confined to summer months in the northern tropics, where in excess of 90% of the annual total is recorded between November and April.

In the north, rain is mostly associated with monsoonal troughs.

The wet season in Queensland is predominantly from January to April when monthly rain falls of 400 mm or more can occur.

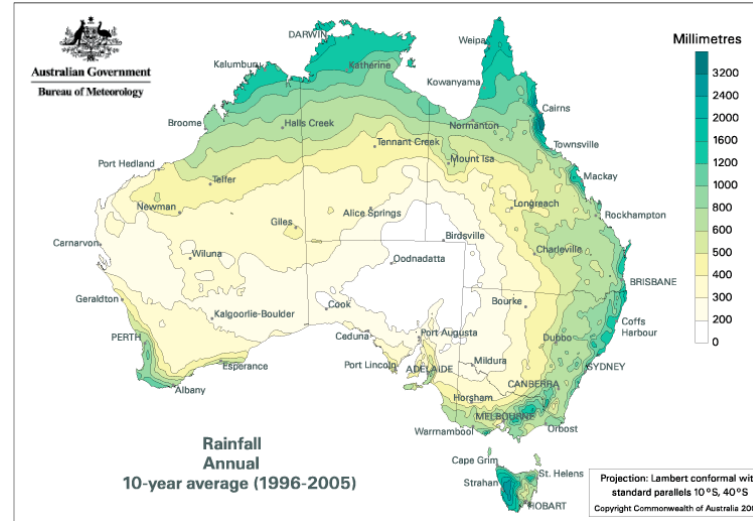
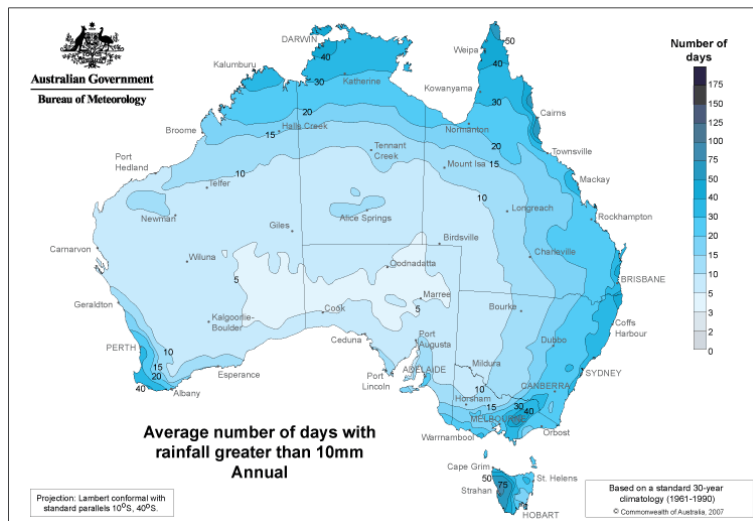
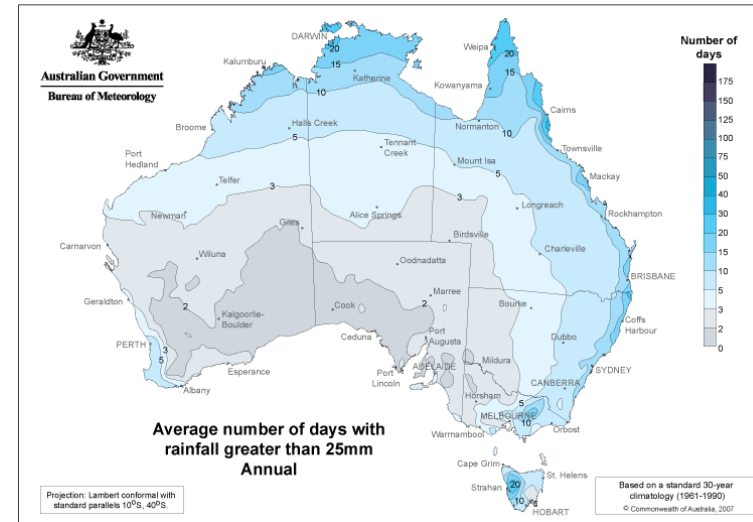
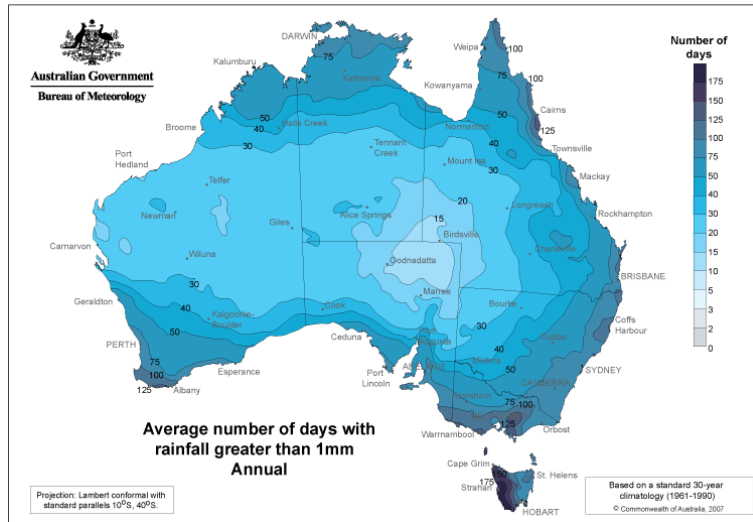
Flooding of low lying areas is likely to occur as a direct result of cyclones and heavy coastal rains. In these instances floodwaters can affect this System on average closing the line for 1 day each year. During a major flooding incident which occurs on average every 5 years, the line could be closed for up to 5 days.

Certain locations on this System can expect greater annual disruption figures namely:-

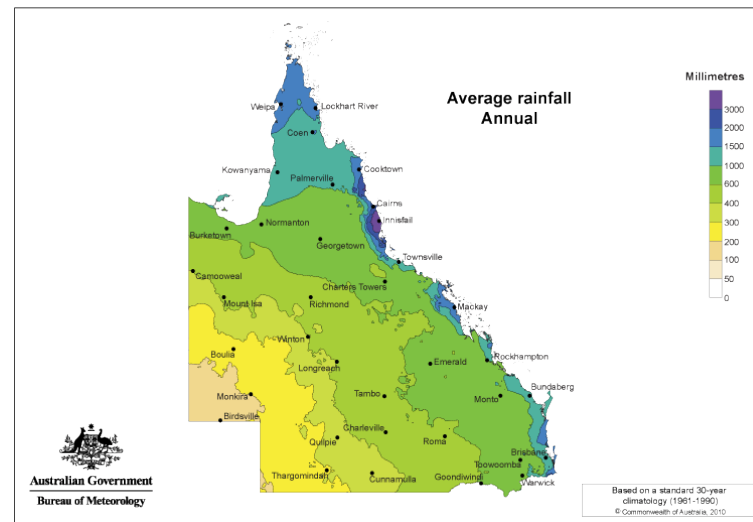
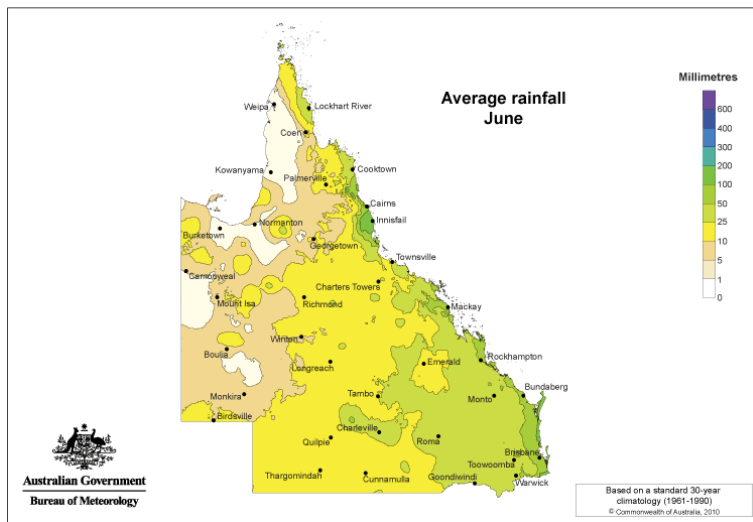
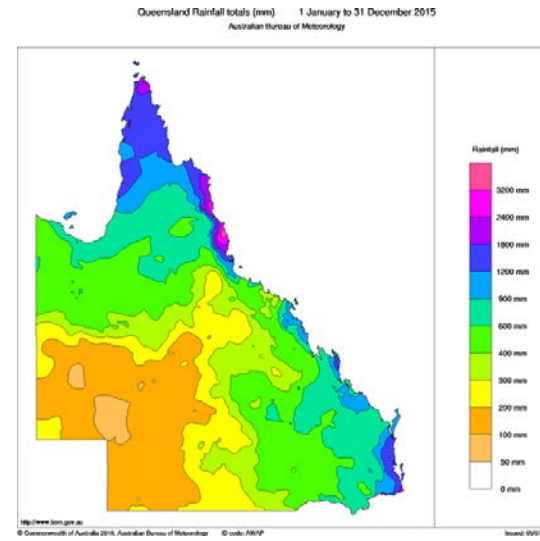
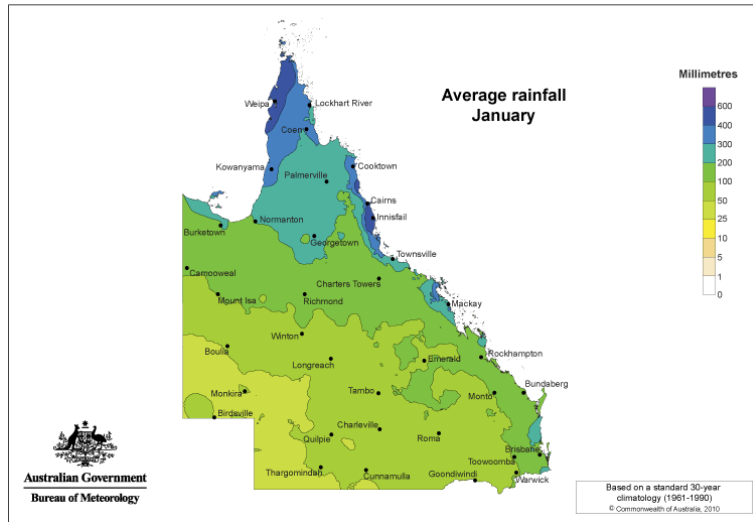
Sarina - Dawlish	2 days / year
Koumala - Illbilbie	2 days / year
Giru Area	3 days / year
Murray Flats, Tully where run-off from the Atherton Tableland can close the track for 6-7 days.	

This is an average figure and closure periods of greater duration are possible in any one year.

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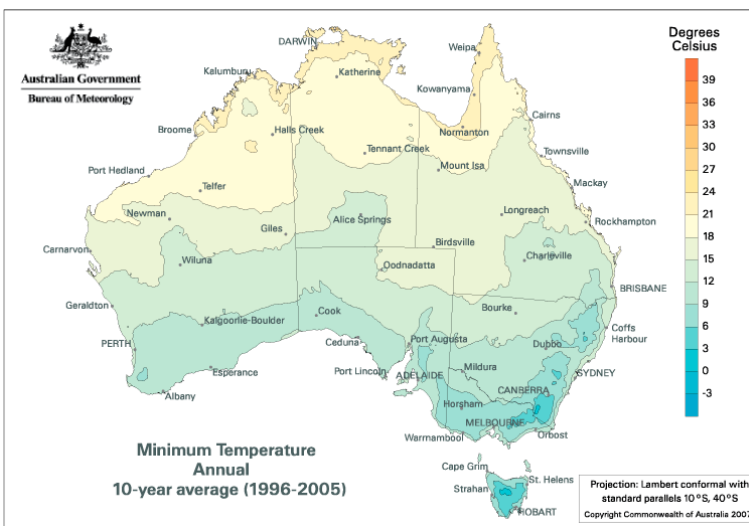
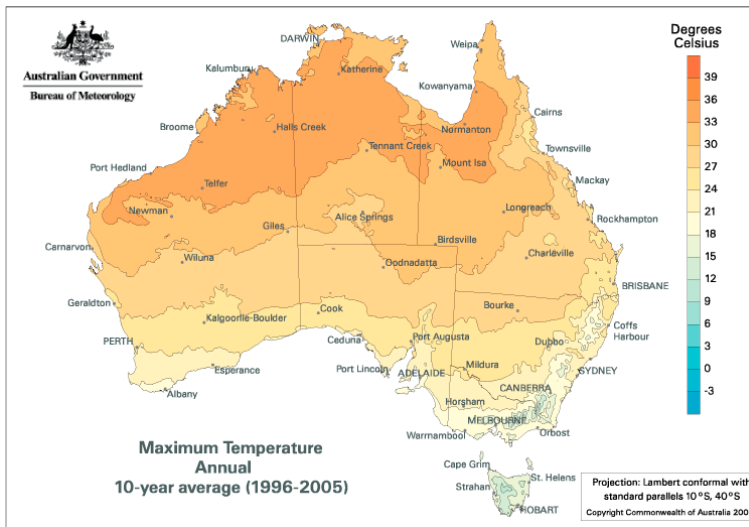


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Temperatures

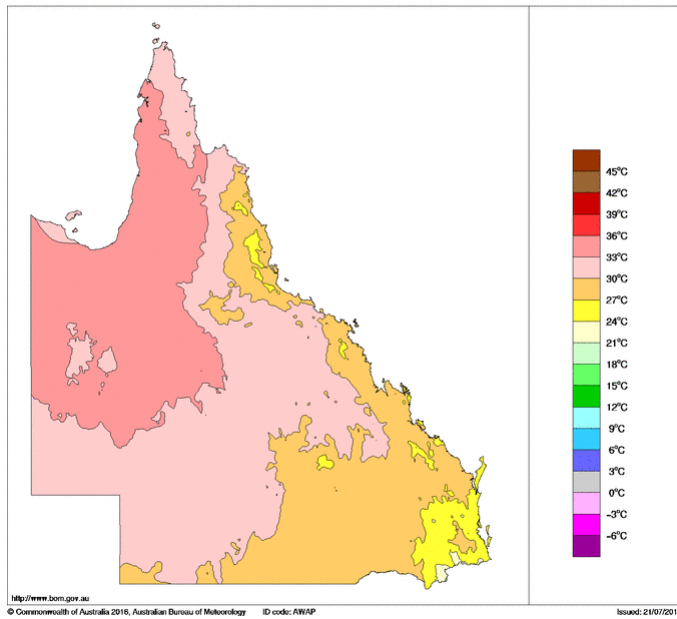
The average annual values of the daytime maximum of the hottest (January) and night-time minimum of the coldest (July) months are indicated on the climatic maps.

During the period of peak temperature, it may be an operational requirement that Line Speed be reduced to minimise the risk of incident (refer Operational Constraints).

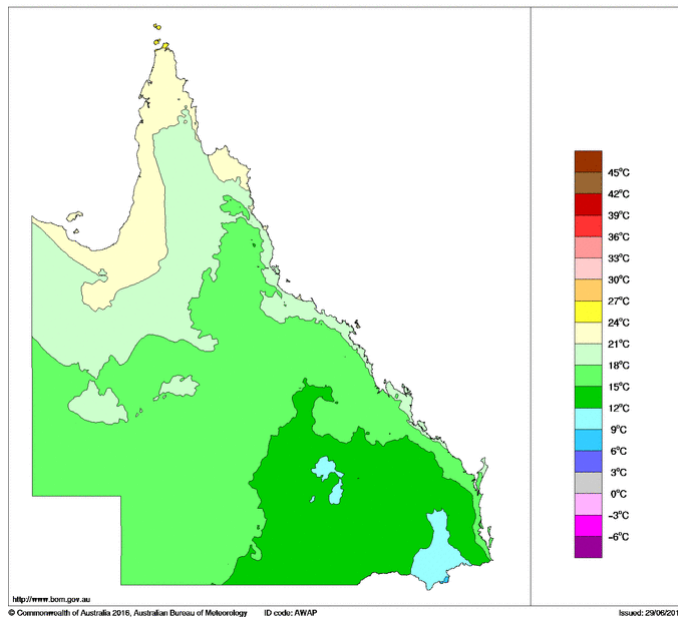


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Maximum Temperature (°C) 1 July 2015 to 30 June 2016
 Australian Bureau of Meteorology



Minimum Temperature (°C) 1 January to 31 December 2015
 Australian Bureau of Meteorology



Description of the Railway

The track (1067 mm gauge) on the North Coast Line System is a mix of nominal 20, 30, 41, 47, 50, 53 and 60 kg/m rail on timber, steel and concrete sleepers.

Information on track, rollingstock, train operations, container operations and special train operations for the safe operation of trains on this System are contained in the MD-10-533 - Operational Route Manual.

Axle Loadings

Maximum axle loads used throughout this document have been determined by either the track configuration or the railway structures below rail. Railway structures were designed for axle loads, axle spacings and vehicle lengths that produce bending moments roughly equivalent to the moments for metric Cooper's loadings as follows :-

Maximum axle load	Metric Cooper's Loading
26 tal	M 220
20 tal	M 160
15.75 tal	M 130

For rollingstock of different configuration, e.g. in respect of axle spacing and vehicle lengths, lesser axle loads and/or speed restrictions may be required before rollingstock may be authorised to operate.

Basic Track Configuration

Basic track configuration is detailed on **APPENDIX B - SCHEMATIC LAYOUTS**.

Rockhampton to Mackay

Rockhampton (elevation 8 m) defines the northern extent of Queensland Rail's electrified network. At Glenmore Junction (elevation 11 m), 3 km north, and across the Fitzroy River, the Yeppoon Branch connects to the North Coast Line.

From Glenmore Junction, the North Coast Line heads north west along the coastal plain, closely following the Bruce Highway to Marlborough (elevation 87 m), crossing the Styx River then onto St Lawrence (elevation 12 m). From St Lawrence the North Coast Line heads north to Kalarka (elevation 11 m), north north west to Sarina (elevation 18 m) in the heart of sugar country then north to Mackay (elevation 9 m).

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Corridor		Rockhampton to Mackay	
Line Code		403, 831, 677, 832, 833, 680, 408, 840, 409, 475	
System		North Coast	
No. of Tracks		1	
Route Km		319.727	
Track Km		319.727	
Electrified		No	
Safeworking System		RCS	
Control Centre		Rockhampton/Townsville	
Crossing Loops	No.	24	
	Location and length	Glenmore (710pp), Parkhurst (923pp), The Caves (712pp), Yaamba (812pp), Glen Geddes (707pp), Kunwarara (698pp), Princhester (717pp), Marlborough (724pp), Kooltandra (700pp) Ogmore (701pp), Wumalgi (715pp), St. Lawrence (697pp), Kalarka (738pp), Elalie (703pp), Carmila (700pp), Orkobie (711pp), Ilbilbie (706pp), Koumala (709pp), Yukan (700pp), Sarina (718pp), Dawlish (705pp), Balberra (708pp), Rosella (724pp), Mackay (810pp)	
Bridges (no of spans./)Length (m)	Timber	No. of Bridges	11
		No. of Spans	81
		Length (m)	400.6
	Steel	No. of Bridges	6
		No. of Spans	67
		Length (m)	943.4
	Concrete	No. of Bridges	86
		No. of Spans	363
		Length (m)	4271.58
Overbridges (No. of Bridges)		Timber	2
		Steel	2
		Concrete	5
Tunnels (no.)/	Length (m)	Number	0
		Length (m)	0
Curves (% of total track)	<80km/h	1.7	
	<60km/h	0.2	
Level Crossings	Public (includes Pedestrian)		82
	Occupation		40
	Tramway / Drawbridge		4
	Fl. Lights		31
	Boom gte		13
Track Structure	Rail Mass		50/53/60kg
	Jointed		CWR
	Sleeper Type		C
Maximum Allowable Axle Load		(tal)	20
Route Speed km/h	Pass		160
	Frt		80/100
	Block		80/100
	Max Container Height - (m)		3.05
Allowable Gross Tonnes p.a.("0 00")		10,000	

Transfer sidings at Yukan (elevation 17 m) provide access between the Aurizon Goonyella System and the North Coast Line. Within the Yard, #1 Transfer Road, #2 Transfer Road and 94 lb Siding as well as the "no run wire" on the North Coast Line main line are electrified.

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Within this section there are a number of Cane Tramway Crossings, be they diamond or drawbridge types (refer Appendix C - Rail / Road Interface Details).

There are twenty-four passing loops on this section namely Glenmore, Parkhurst, The Caves, Yaamba, Glen Geddes, Kunwarara, Princhester, Marlborough, Kooltrandra, Ogmore, Wumalgi, St Lawrence, Kalarka, Elalie, Carmilla, Orkobie, Ilbilbie, Koumala, Yukan, Sarina, Dawlish, Balberra, Rosella and Mackay.

Track structure is a mix of nominal 50, 53 and 60 kg/m rail on concrete sleepers. The maximum allowable axle load is 20 tal.

The maximum allowable speed is 100 km/h. Conditions apply for freight and block trains, refer STD/0071/SWK - Operational Route Manual.

The maximum grade (not compensated for horizontal alignment) that a northbound (Down) train will encounter is 1 in 49 near Kalarka whilst for an southbound (Up) train the maximum grade is 1 in 49 also near Kalarka.

Existing minimum nominal horizontal curve radii are as follows :-

running line 100 m

Fencing along this corridor complements adjacent land usage and is at the following standard, poor (20 %), medium (40 %) and good (40 %). Fencing will be maintained at its current standard.

Mackay to Durroburra

From Mackay (elevation 9 m), the railway passes under Archibald Street, past the angle to the Marian Branch, then under Nebo Road before crossing the Pioneer River and on to Erakala (elevation 11 m) and the angle to the Mackay Harbour Branch.

From Erakala, the North Coast Line continues north west to Durroburra, just north of Merinda (elevation 8 m). The section Durroburra to Kaili on the North Coast Line is dealt with in the Newlands System Information Pack.

The Branch Line to the Bowen CokeWorks / Jetty connects with North Coast Line between Mookarra and Merinda. The configuration of this branch is such that it can only be serviced from Merinda.

Within this section there are a number of Cane Tramway Crossings, be they diamond or drawbridge types (refer Appendix C - Rail / Road Interface Details).

There are thirteen passing loops on this section namely Farleigh, Aminungo, Kuttabel, Mount Ossa, Calen, Yalboroo, Bloomsbury, Thoopara, Proserpine, Bubialo, Longford Creek, Mookarra and Merinda.

Track structure is a mix of nominal 50, 53 and 60 kg/m rail on concrete sleepers. The maximum allowable axle load is 20 tal.

The maximum allowable speed is 100 km/h.

The maximum grade (not compensated for horizontal alignment) that a southbound (Up) train will encounter is 1 in 44 whilst for an northbound (Down) train, the maximum grade is 1 in 44, both grades located around Kuttabel.

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Existing minimum nominal horizontal curve radii are as follows :-

running line 240 m

Fencing along this corridor complements adjacent land usage and is at the following standard, poor (15 %), medium (50 %) and good (35%). Fencing will be maintained at its current standard.

Corridor		Mackay to Durroburra	
Line Code		836, 685, 843, 844, 849	
System		North Coast	
No. of Tracks		1	
Route Km		187.863	
Track Km		187.863	
Electrified		No	
Safeworking System		RCS	
Control Centre		Townsville	
Crossing Loops		No. 13	
Location and length		Farleigh (702pp), Aminungo (750pp), Kuttabul (752pp), Mount Ossa (703pp), Calen (705pp), Yalboroo (863pp), Bloomsbury (708pp), Thoopara (716pp), Proserpine (697pp), Bubialo (700pp), Longford Creek (805pp), Mookarra (845pp), Merinda (702pp)	
Bridges (no of spans.)/Length (m)	Timber	No. of Bridges	5
		No. of Spans	33
		Length (m)	175.5
	Steel	No. of Bridges	7
		No. of Spans	61
		Length (m)	780.4
	Concrete	No. of Bridges	58
		No. of Spans	226
		Length (m)	3270.7
Overbridges (No. of Bridges)		Timber 2	
		Steel 2	
		Concrete 5	
Tunnels (no.)/	Length (m)	Number	0
		Length (m)	0
Curves (% of total track)	<80km/h		3
	<60km/h		1
Level Crossings	Public (includes Pedestrian)		51
	Occupation		37
	Tramway / Drawbridge		5
	Fl. Lights		18
	Boom gte		8
Track Structure	Rail Mass		50/53/60kg
	Jointed		CWR
	Sleeper Type		C
Maximum Allowable Axle Load (tal)			20
Route Speed km/h	Pass		140
	Frt		100
	Block		100
Max Container Height - (m)			3.05
Allowable Gross Tonnes p.a. ("0 00")			10,000

Kali to Townsville

From Kaili, the North Coast Line follows the Bruce Highway to Home Hill (elevation 11 m), crosses the Burdekin River into Ayr (elevation 10 m) and continues to Nome (elevation 19 m).

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The 10 km section from Nome to Stuart (elevation 18 m) is double track. Stuart is the junction of the North Coast Line and the Mt Isa Line. From Stuart the double track continues into Townsville stopping south of Boundary Street.

Crossing over Boundary Street the single line continues east to South Yard and Townsville Jetty or west to Townsville Station, constructed over the Ross River.

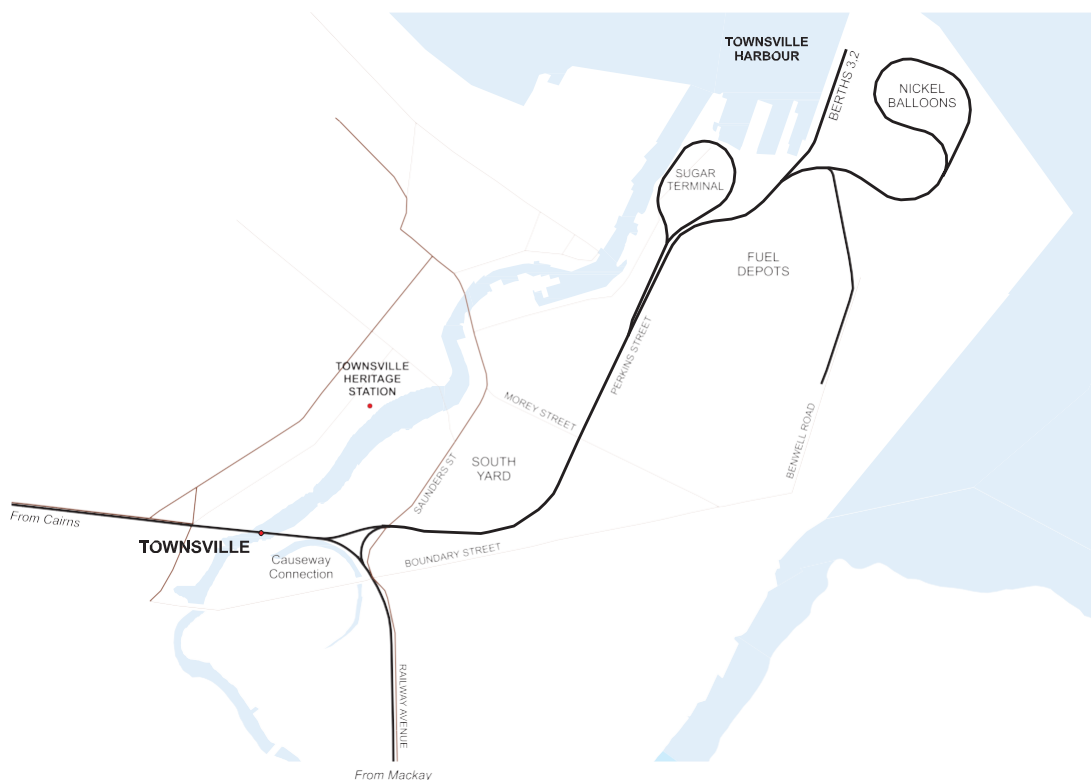
This section of track is vulnerable to flooding during the wet season around Guthalungra and between Giru and Cromarty.

Corridor		Kaili to Home Hill	Home Hill to Stuart	Stuart to Townsville	
Line Code		312	689, 480, 481	891	
System		North Coast	North Coast	North Coast	
No. of Tracks		1	1 (Nome)/ 2	2	
Route Km		85.989	81.389	8.505	
Track Km		85.989	91.852	17.01	
Electrified		No	No	No	
Safeworking System		RCS	RCS	RCS	
Control Centre		Townsville	Townsville	Townsville	
Crossing Loops	No.	8	6	0	
	Location and length	Kaili (1414pp), Wathana (701pp), Wilmington (692pp), Guthalungra (701pp), Gumlu (698pp), Bobawaba (720pp), Inkerman (703pp), Home Hill (700pp)	Ayr, Pioneer (698pp), Barratta (700pp), Giru (832pp), Cromarty (701pp), Storth (698pp)		
Bridges (no of spans.) / Length (m)	Timber	No. of Bridges	2	3	0
		No. of Spans	24	17	0
		Length (m)	118.3	83.4	0
	Steel	No. of Bridges	9	9	1
		No. of Spans	110	155	13
		Length (m)	880.4	2131.6	158.6
	Concrete	No. of Bridges	20	26	3
		No. of Spans	86	107	46
		Length (m)	991.9	1286.7	590.5
Overbridges (No. of Bridges)		Timber	0	0	0
		Steel	0	0	0
		Concrete	0	0	0
Tunnels (no.) /	Length (m)	Number	0	0	0
		Length (m)	0	0	0
Curves (% of total track)	<80km/h	1	4	4	
	<60km/h	0	1	1	
Level Crossings	Public (includes Pedestrian)		22	33	7
	Occupation		13	8	0
	Tramway / Drawbridge		0	3	0
	Fl. Lights		9	13	1
	Boom gte		1	4	4
Track Structure	Rail Mass		53kg (Passing loops), 60kg (Main Line)	50/53/60kg	47kg
	Jointed		CWR	CWR	CWR
	Sleeper Type		C	C	C
Maximum Allowable Axle Load (tal)		20	20	20	
Route Speed km/h	Pass		140	140	100
	Frt		100	100/80	80
	Block		100	100/80	80
	Max Container Height - (m)		3.05	3.05	3.05
Allowable Gross Tonnes p.a. ("000")		10,000	14,000	16,000	

There are fourteen passing loops on this section namely Kaili, Wathana, Wilmington, Guthalungra, Gumlu, Bobawaba, Inkerman, Home Hill, Ayr, Pioneer, Barratta, Giru, Cromarty and Storth.

Track structure is 50, 53 and 60 kg/m rail on concrete sleepers.

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The maximum allowable axle load is 20 tal.
 The maximum allowable speed is 100 km/h.

The maximum grade(not compensated for horizontal alignment) that a southbound (Up) train will encounter is 1 in 44 (near Giru) whilst for an northbound (Down) train the maximum grade is 1 in 50 (near Guthalungra).

Existing minimum nominal horizontal curve radii are as follows:-

running line 240 m

Fencing along this corridor complements adjacent land usage and is at the following standard, poor (25 %), medium (45 %) and good (30 %). Fencing will be maintained at its current standard.

Townsville to Cairns

From Townsville Station, (elevation 3 m) trains continue west to Garbutt (elevation 5 m).

At Garbutt, the railway heads north west, bounded in the west by the Paluma Range and the east by the coastline to Bambaroo (elevation 15 m). From here, the railway heads north to Ingham (elevation 12 m), through the Lumholz National Park and on to Cardwell (elevation 7 m).

Yabulu, 21 kilometres north of Garbutt, is the junction of the Cobarra Balloon Loop. This loop services the local refinery.

From Cardwell, the railway heads up to Tully (elevation 16 m) crossing the Murray River on the way. Wet season inundation of the railway is common in this area, particularly over the Murray River Flats, where the track can be closed for between 6 - 7 days.

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Corridor			Townsville to Purono	Purono to Portsmith	Portsmith to Cairns
Line Code			325, 326, 852, 853, 142, 333, 888, 107, 651	854, 855, 857, 417, 315, 478	316
System			North Coast	North Coast	North Coast
No. of Tracks			1	1	1
Route Km			30.214	310.375	2.853
Track Km			30.214	310.375	2.853
Electrified			No	No	No
Safeworking System			RCS	DTC	RCS
Control Centre			Townsville	Townsville	Townsville
Crossing Loops			2	22	1
No.					
Location and length			Garbutt (805tp), Deeragun (800tp)	Purono (712tp), Kurukan (701tp), Rollingstone (703tp), Mutamee (699tp), Bambaroo (706tp), Pombel (701tp), Ingham (705tp), Hinchinbrook (701tp), Conn (701tp), Cardwell (731tp), Kennedy (688tp), Bilyana (701tp), Tully (506m), El Arish (688tp), Silkwood (754tp), Boogan (729tp), Innisfail (717tp), Waugh (657tp), Babinda (678tp), Deeral (718tp), Alooomba (787tp), Kamma (725tp)	Portsmith (900pp), Cairns (439pp)
Bridges (no of spans./)Length (m)	Timber	No. of Bridges	0	70	0
		No. of Spans	0	454	0
		Length (m)	0	2344	0
	Steel	No. of Bridges	1	10	0
		No. of Spans	5	42	0
		Length (m)	36.5	961	0
	Concrete	No. of Bridges	12	82	2
		No. of Spans	79	382	2
		Length (m)	670.1	3980	22.7
Overbridges (No. of Bridges)			Timber	0	2
			Steel	0	0
			Concrete	0	0
Tunnels (no./)			Length (m)	Number	0
				0	0
				0	0
Curves (% of total track)			<80km/h	6	8
			<60km/h	6	3
Level Crossings			Public (includes Pedestrian)	26	146
			Occupation	4	133
			Tramway / Drawbridge	0	27
			Fl. Lights	5	35
			Boom gte	12	0
Track Structure			Rail Mass	41/47/53kg	41/47/53kg
			Jointed	CWR	CWR/LWR
			Sleeper Type	100% S	100% S
Maximum Allowable Axle Load (tal)			20	20	20
Route Speed km/h			Pass	100	100/80
			Frnt	100/80	80
			Block	100/80	80
Max Container Height- (m)			3.05	3.05	3.05
Allowable Gross Tonnes p.a. ("0 00")			11,000	3,000	3,000

From Tully, the railway heads north north east skirting the Walter Hill Range to Innisfail (elevation 8 m) located on the banks of the Johnstone and South Johnstone Rivers. From here, north west past Mt Bartle Freer to Babinda (elevation 11 m), through the Woorroonoran National Park and on into Cairns (elevation 3 m).

There are twenty six passing loops on this section namely Garbutt, Deeragun, Purono, Kurukan, Rollingstone, Mutarnee, Bambaroo, Pombel, Ingham, Hinchinbrook, Conn, Cardwell, Kennedy, Bilyana, Tully, El Arish, Silkwood, Boogan, Innisfail, Waugh, Babinda, Deeral, Alooomba, Kamma, Portsmith and Cairns.

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Track structure is 41, 47 and 53 kg/m rail on steel sleepers. There is one section of concrete sleepers track over the Mulgrave River, south of Gordonvale.

The maximum allowable axle load is 20 tal.

The maximum allowable speed for locomotive hauled stock Townsville to Purono is 100 km/h, beyond Purono is 80 km/h with Tilt trains 100 km/h.

The maximum grade(not compensated for horizontal alignment) that a southbound (Up) train will encounter is 1 in 33 whilst for an northbound (Down) train the maximum grade is 1 in 33, in the El Arish area.

Existing minimum nominal horizontal curve radii are as follows :-

running line 140 m

40% of this corridor is fenced. Fencing complements adjacent land usage and is at the following standard, poor (13%), medium (74%) and good (13 %). Fencing will be maintained at its current standard.

Glenmore to Yeppoon

After leaving the North Coast Line at Glenmore (elevation 11 m), the railway heads east skirting the Mount Archer National Park then north east to Yeppoon (elevation 10 m) on the shores of Rosslyn Bay.

There are four passing loops on this section namely North Rockhampton, Lakes Creek, Fields Siding and Yeppoon.

Track structure is a mix of nominal 20, 31 and 41 kg/m rail on timber sleepers.

The maximum allowable axle load is 15.75 tal to Fields Siding (10,930 km) then 10.62 tal.

The maximum allowable speed is 60 km/h to Fields Siding, then 50 km/h to 34 km and then 40 km/h.

The maximum grade(not compensated for horizontal alignment) that a westbound (Up) train will encounter is 1 in 50 whilst for an eastbound (Down) train the maximum grade is 1 in 50.

Existing minimum nominal horizontal curve radii are as follows :-

running line 140 m

Fencing along this corridor complements adjacent land usage and is at the following standard, poor (27%) and good (73 %). Fencing will be maintained at its current standard.

Corridor	Glenmore to Nerimbera	Nerimbera to Yeppoon
Line Code	410	411
System	North Coast	North Coast
No. of Tracks	1	1

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Route Km		9.243	44.387
Track Km		9.243	44.387
Electrified		No	No
Safeworking System		RCS/S&T	S&T
Control Centre		Rockhampton	Rockhampton
Crossing Loops	No.	3	2
	Location and length	North Rockhampton (443tp), Lakes Creek (332m), Nerimbera	Fields Meat Works Siding (150m) Yeppoon (282m)
Bridges (no of spans./)Length (m)	Timber	No. of Bridges	6
		No. of Spans	65
		Length (m)	340.7
	Steel	No. of Bridges	0
		No. of Spans	0
		Length (m)	0
	Concrete	No. of Bridges	0
		No. of Spans	0
Length (m)		0	
Overbridges (No. of Bridges)		Timber	3
		Steel	0
		Concrete	2
Tunnels (no./)	Length (m)	Number	0
		Length (m)	0
		Length (m)	0
Curves (% of total track)	<80km/h	22	22
	<60km/h	9	10
Level Crossings	Public (includes Pedestrian)	12	18
	Occupation	3	20
	Tramway / Drawbridge	0	0
	Fl. Lights	3	1
	Boom gte	1	0
Track Structure	Rail Mass	41kg	20/31/41kg
	Jointed	SWR	SWR
	Sleeper Type	T	T
Maximum Allowable Axle Load (tal)		15.75	10.62
Route Speed km/h	Pass	60	60/50/40
	Frnt	60	60/50/40
	Block		
	Max Container Height - (m)	2.65	2.65
Allowable Gross Tonnes p.a. ("0 00")		1,000	500

Mackay to Marian

After leaving the North Coast Line at Mackay (elevation 9 m) the railway heads west paralleling the Peak Downs Highway to Walkerston (elevation 21 m). The railway then continues west to Marian (elevation 42 m). This railway has restricted access outside the sugar season.

There are no passing loops on this section railway.
 Track structure is nominal 41 and 53 kg/m rail on timber sleepers.
 The maximum allowable axle load is 15.75 tal.
 The maximum allowable speed is 50 km/h.

The maximum grade(not compensated for horizontal alignment) that a westbound (Up) train will encounter is 1 in 50 (20 kp) whilst for an eastbound (Down) train the maximum grade is 1 in 99 (20 kp).

Existing minimum nominal horizontal curve radii are as follows :-

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running line 300 m

This corridor runs through canefields and is generally unfenced.

Corridor		Mackay to Marian	
Line Code		684, 117	
System		North Coast	
No. of Tracks		1	
Route Km		23.26	
Track Km		23.654	
Electrified		No	
Safeworking System		S&T	
Control Centre		Townsville	
Crossing Loops	No.	0	
	Location and length		
Bridges (no of spans./)Length (m)	Timber	No. of Bridges	9
		No. of Spans	33
		Length (m)	154.6
	Steel	No. of Bridges	0
		No. of Spans	0
		Length (m)	0
	Concrete	No. of Bridges	1
		No. of Spans	3
		Length (m)	48
Overbridges (No. of Bridges)	Timber	0	
	Steel	0	
	Concrete	0	
Tunnels (no.)/	Length (m)	Number	0
		Length (m)	0
Curves (% of total track)	<80km/h	6	
	<60km/h	1	
Level Crossings	Public (includes Pedestrian)	20	
	Occupation	12	
	Tramway / Drawbridge	7	
	Fl. Lights	4	
	Boom gte	0	
Track Structure	Rail Mass	41/ 53kg	
	Jointed	SWR	
	Sleeper Type	T	
Maximum Allowable Axle Load (tal)		15.75	
Route Speed km/h	Pass	50	
	Frnt	50	
	Block		
Max Container Height - (m)		2.65	
Allowable Gross Tonnes p.a.("0 00")		1.000	

Erakala to Mackay Harbour

The angle at Erakala (elevation 11 m) allows traffic from the north and the south to reach the port facilities at Mackay Harbour.

There are no passing loops on this section railway.

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Track structure is nominal 41 kg/m rail on timber sleepers. The maximum allowable axle load is 15.75 tal.

The maximum allowable speed is 80 km/h with block trains limited to 60 km/h.

The maximum grade(not compensated for horizontal alignment) that a westbound (Up) train will encounter is 1 in 140 whilst for an eastbound (Down) train the maximum grade is 1 in 152.

Existing minimum nominal horizontal curve radii are as follows :-

running line 300 m

This corridor runs through canefields and is generally unfenced.

Corridor		Erkala to Mackay Harbour	
Line Code		682, 116	
System		North Coast	
No. of Tracks		1	
Route Km		10.723	
Track Km		11.228	
Electrified		No	
Safeworking System		RCS	
Control Centre		Townsville	
Crossing Loops	No.	0	
	Location and length		
Bridges (no of spans./)Length (m)	Timber	No. of Bridges	0
		No. of Spans	0
		Length (m)	0
	Steel	No. of Bridges	0
		No. of Spans	0
		Length (m)	0
	Concrete	No. of Bridges	3
		No. of Spans	5
		Length (m)	75
Overbridges (No. of Bridges)	Timber	0	
	Steel	0	
	Concrete	5	
Tunnels (no.)/	Length (m)	Number	0
		Length (m)	0
Curves (% of total track)	<80km/h	18	
	<60km/h	0	
Level Crossings	Public (includes Pedestrian)	7	
	Occupation	7	
	Tramway / Drawbridge	0	
	Fl. Lights	2	
Track Structure	Boom gte	0	
	Rail Mass	41kg	
	Jointed	CWR	
Sleeper Type		T	
Maximum Allowable Axle Load (tal)		15.75	
Route Speed km/h	Pass	80	
	Frt	80	
	Block	60	
Max Container Height - (m)		2.65	
Allowable Gross Tonnes p.a. ("0 00")		3,000	

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Bowen Jetty and Bowen Cokeworks

This branch line can only be serviced from the north.

There are no passing loops on this section railway.

Track structure is nominal 31 and 41 kg/m rail on timber sleepers.

Corridor		Bowen Jct to Bowen Jetty/Bowen Cokeworks	
Line Code		687	
System		North Coast	
No. of Tracks		Yard	
Route Km		4.503	
Track Km		4.503	
Electrified		No	
Safeworking System		Yard Control	
Control Centre		Shunter	
Crossing Loops	No.	0	
	Location and length		
Bridges (no of spans./)Length (m)	Timber	No. of Bridges	0
		No. of Spans	0
		Length (m)	0
	Steel	No. of Bridges	1
		No. of Spans	1
		Length (m)	20
	Concrete	No. of Bridges	0
		No. of Spans	0
		Length (m)	0
Overbridges (No. of Bridges)		Timber	0
		Steel	0
		Concrete	0
Tunnels (no.)/	Length (m)	Number	0
		Length (m)	0
Curves (% of total track)	<80km/h	20	
	<60km/h	10	
Level Crossings	Public (includes Pedestrian)		5
	Occupation		0
	Tramway / Drawbridge		0
	Fl. Lights		2
	Boom gte		0
Track Structure	Rail Mass		41kg / 60lb
	Jointed		
	Sleeper Type		T
Maximum Allowable Axle Load (tal)			15.75
Route Speed km/h	Pass		25
	Frt		25
	Block		25
Max Container Height - (m)			2.65
Allowable Gross Tonnes p.a.("0 00")			-

The maximum allowable axle load is 15.75 tal.

The maximum allowable speed is 25 km/h.

The maximum grade(not compensated for horizontal alignment) that a westbound (Up) train will encounter is 1 in 98 whilst for an eastbound (Down) train the maximum grade is 1 in 66.

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Existing minimum nominal horizontal curve radii are as follows :-

running line 300 m

Fencing along this corridor is virtually non-existent.

Townsville to Townsville Jetty

The Townsville Jetty branch is predominantly double track and features a number of balloon loops and sidings serving industry at the Port of Townsville.

There are no passing loops on this section of the railway; however crossovers permit ease of operation from one track to the other.

Corridor		Townsville to Townsville Jetty	
Line Code		144, 137, 489, 136, 477, 490, 487	
System		North Coast	
No. of Tracks		2	
Route Km		8.247	
Track Km		9.419	
Electrified		No	
Safeworking System		RCS	
Control Centre		Townsville	
Crossing Loops	No.	0	
	Location and length		
Bridges (no of spans.)/Length (m)	Timber	No. of Bridges	0
		No. of Spans	0
		Length (m)	0
	Steel	No. of Bridges	0
		No. of Spans	0
		Length (m)	0
	Concrete	No. of Bridges	0
		No. of Spans	0
		Length (m)	0
Overbridges (No. of Bridges)		0	
		0	
		0	
Tunnels (no.)/	Length (m)	Number	0
		Length (m)	0
Curves (% of total track)	<80km/h	20	
	<60km/h	10	
Level Crossings	Public (includes Pedestrian)	7	
	Occupation	7	
	Tramway / Drawbridge	0	
	Fl. Lights	1	
	Boom gte	0	
Track Structure	Rail Mass	60 / 50 / 47 / 41kg (predominately)	
	Jointed	CWR/LWR/SWR/B	
	Sleeper Type	T, S, C	
Maximum Allowable Axle Load (tal)		20	
Route Speed km/h	Pass	15	
	Frt	15	
	Block	15	
Max Container Height - (m)		3.05	
Allowable Gross Tonnes p.a. ("0 00")		12,000	

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Track structure is nominal 41, 47 and 50 kg/m rail on a mix of timber and steel sleepers.

The maximum allowable axle load is 20 tal.
The maximum allowable speed is 15 km/h.

The maximum grade (not compensated for horizontal alignment) that a westbound (Up) train will encounter is 1 in 114 whilst for an eastbound (Down) train the maximum grade is 1 in 150.

Existing minimum nominal horizontal curve radii are as follows :-

running line 100 m

This corridor is not fenced.

Speeds and Curves

Horizontal radius for new or upgrade works on this System is as follows :-

running line	
2170 m	160 km/h running
1910 m	150 km/h running
1662 m	140 km/h running
1432 m	130 km/h running
1221 m	120 km/h running
1026 m	110 km/h running
848 m	100 km/h running
687 m	90 km/h running
542 m	80 km/h running
balloon loop	300 m minimum radius
siding and depot	140 m

Description of the Track

The track on this system is a mix of nominal 60 kg/m, 53 kg/m, 50 kg/m, 47 kg/m, 41 kg/m and 31 kg/m rail and timber, steel and concrete sleepers on crushed rock ballast.

Speeds through the curved leg of turnouts are governed by the angle of that turnout ie.

1 in 12 25 km/h
1 in 16 50 km/h
1 in 25 80 km/h

In general, curves (with the exception of turnout curves) are transitioned.

Preliminary Track Data and Grade Diagrams for the following major route are included in Appendix E.

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[GRADE]

- A+ >30 tonne axle loads
- A 22-30 tonne axle loads
- B 20-22 tonne axle loads
- C 18-20 tonne axle loads
- D 15.5-18 tonne axle loads
- E <15.5 tonne axle loads



Operational Constraints - Infrastructure

During the summer months of high temperatures, hot weather precautions for track stability are observed to reduce the risk of incident in accordance with Safety Management Standard **MD-10-143 Hot Weather Precautions for Track Stability**, namely :-

Air Temperature 38°C and above	-	On timber sleepers track, restrict trains to 60 km/h (#)
		On concrete sleepers track, restrict all trains to 120 km/h
Air Temperature 40°C and above	-	On timber sleepers track, restrict trains to 40 km/h (#)
		On concrete sleepers track, restrict all trains to 60 km/h

(#) Steel sleepers track and timber sleepers track with interspersed steel sleepers shall be regarded as equivalent to timber sleepers track for track stability.

Speed restrictions may also be put in place after maintenance activities in accordance with Queensland Rail Safety Standards.

The extent of restriction will depend upon the type of maintenance activity and risk of track misalignments.

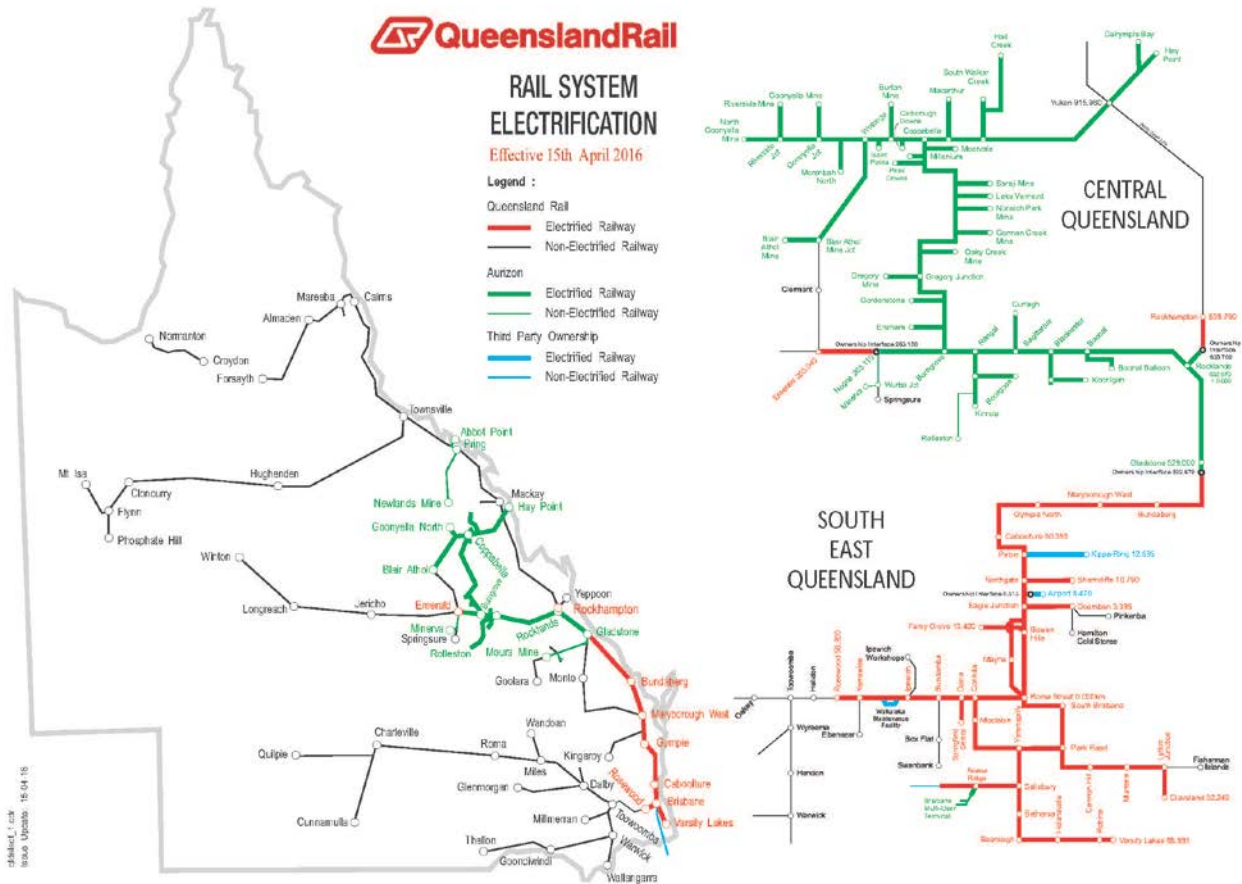
Force Majeure Events will also see the imposition of speed restrictions, the extent and severity of the restrictions being dependent on the event.

Overhead Line Equipment

The North Coast Line System north of Rockhampton is not electrified.

Certain sections of the Aurizon Network are electrified.

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Trackside Detection Equipment

Trackside detection equipment is installed on this system to protect below rail infrastructure and rollingstock from incident.

A summary of locations / equipment type / reporting functions can be found in Appendix I.

Track circuits and axle counters are used for train detection within this System.

Weighbridges

There is one weighbridge on this System located as follows :-

Nickel Balloon Loop in motion / trade certified
 (Townsville Jetty)

Operational Systems & Train Control

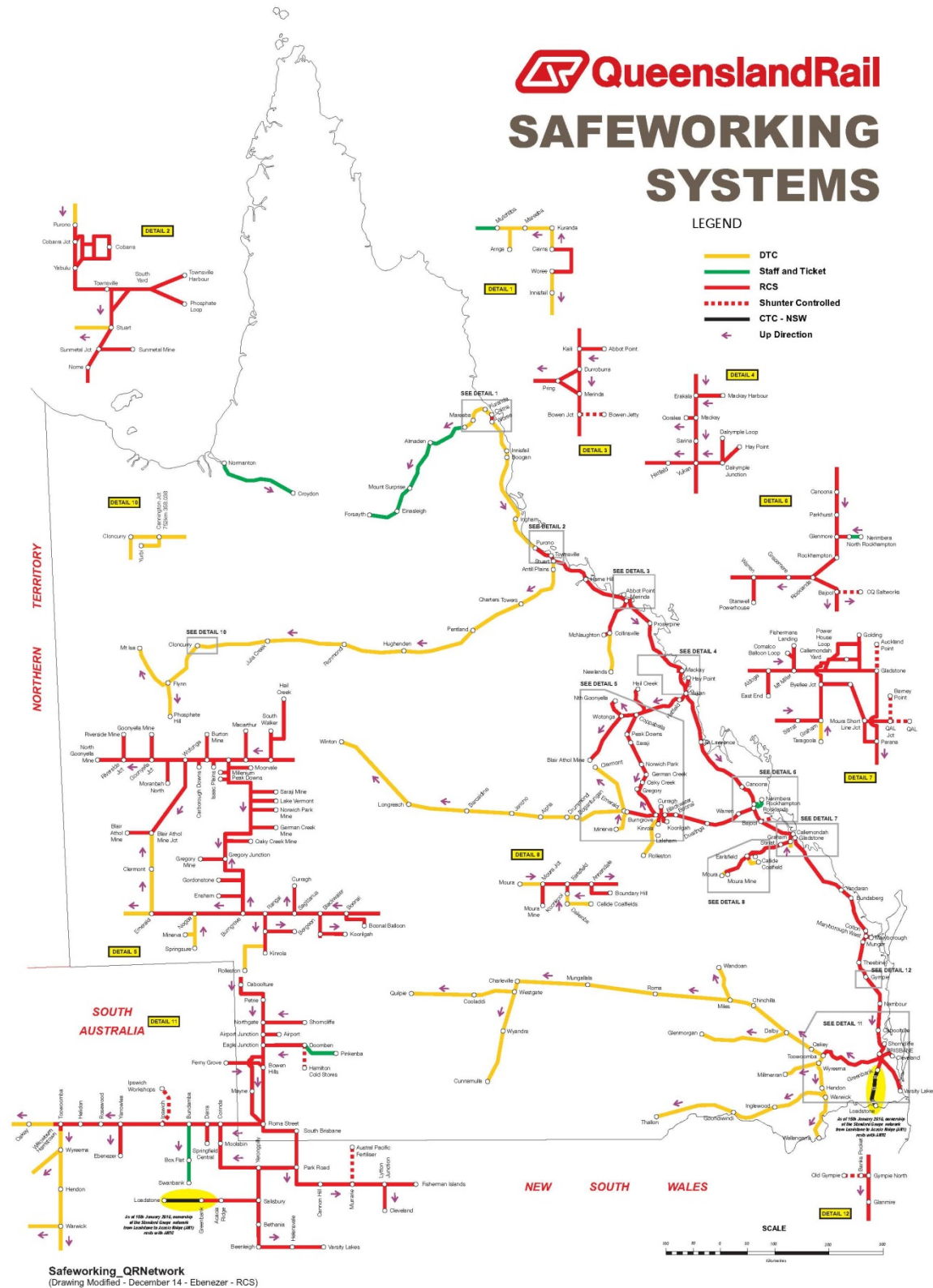
The North Coast Line System is controlled by Remote Control Signalling (RCS) in conjunction with Automatic Train Protection (ATP) to Purono and Direct Traffic Control (DTC) from Purono to Woree. Woree to Cairns is RCS.

Staff and Ticket operates the Yeppoon Branch between North Rockhampton and Yeppoon.

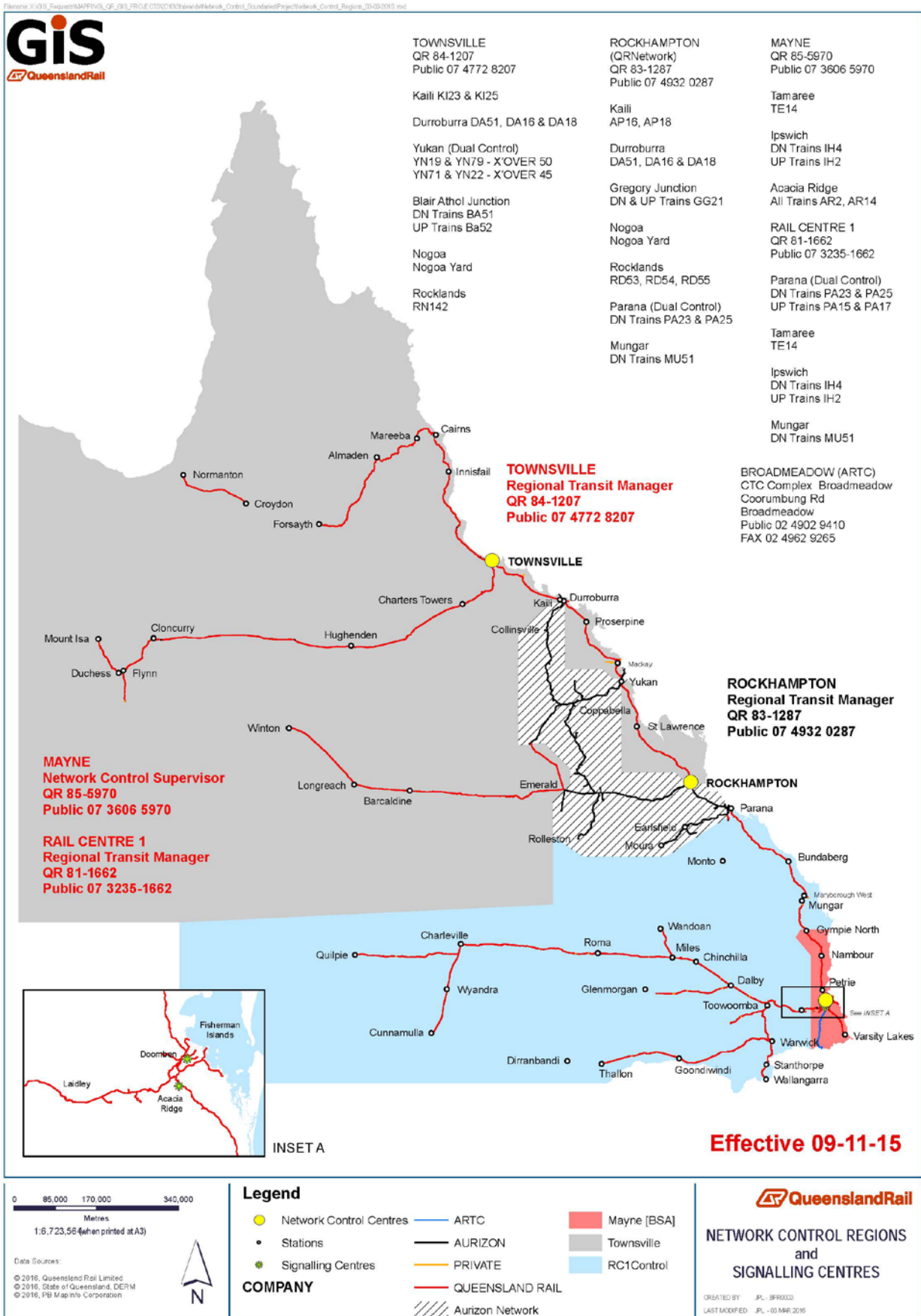
The Bowen Jetty Branch is “shunter controlled”.

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Train Control for the North Coast Line, from Rockhampton to Parkhurst and Glenmore to Yeppoon rests with Rockhampton Control whilst Parkhurst to Cairns including Branches rest with Townsville Control.



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Information Systems

ViziRail is the key software system designed as a tool for use in integrated scheduling, possession planning, monitoring and reporting on the Queensland Rail network.

Functionality includes modules:

- Train notices
- Actual train running (ATR)
- Incidents
- Train consists (Train Builder)
- Speed restrictions
- Rollingstock allocations
- Rollingstock maintenance
- Fresh turnouts
- Planning graphs
- Scheduling enhancements
- Possession enhancements
- OTIS (Operational to Information Systems) - which converts train steps to actual arrival and departure train information.

Operational Constraints - Rollingstock

All new rollingstock requires to be accepted via the Rollingstock Authorisation Process, rollingstock which conform with Drawings 2236 may operate in an unrestricted manner on main lines.

For rollingstock to conform with Drawing Nos. 2236 the static rollingstock profile must be within the diagram, refer **APPENDIX H - Rollingstock Gauges**. As well as the static component, dynamic effects need to be considered and these effects are contained within the Rollingstock Interface Standards.

Rollingstock not conforming to these drawings may be accepted via the Rollingstock Authorisation Process and may be operated subject to constraints / limitations imposed as a result of the Authorisation Process.

Potential railway operators should ensure that they have the latest revision of these drawings before the planning and construction of rollingstock.

Communications

Communications on the North Coast Line between Driver and Controller is via a UHF radio system (Train Control Radio - TCR) utilising a number of Queensland Rail channels and frequencies. Tranceivers “auto” switch channels to suit geographical location. Frequency specification and coverage details are available as part of the “Access Enquiry Process”. Control phones are located at Staff Stations only.

Access to the Maintenance Supervisory Radio System (MSR) can be gained by using Queensland Rail telephone extensions depending on location or UHF radio system utilising Queensland Rail channels.

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In addition, all current locomotives (including Multiple Units and Miscellaneous Vehicles such as Rail Motors) carry and all units new to the system will be required to carry a UHF radio operating on Queensland Rail Channel 1. This provides on-board and wayside communications including end to end, train to train and train to track gangs over a distance on average of 8 - 10 km.

Communications on board locomotives must conform to **Queensland Rail's Safety and Security Standard MD-10-86 - Mobile Voice Radio Communications Systems.**

Sectional Running Times

The sectional running times, expressed as minutes, for various types of trains currently operating on the system are contained in **APPENDIX F**.

The sectional running times are "Pass to Pass" times for a running move and do not reflect acceleration and deceleration characteristics of trains.

Sectional running times are provided for two (2) types of locomotive-hauled trains - passenger and container (inter-modal).

Proposed train configurations would need to be confirmed by the relevant operator against the infrastructure constraints to determine if the sectional running times can be achieved. If the sectional running times cannot be achieved then different arrangements, including for access charges, may need to be negotiated as part of the access agreement negotiations.

Changes to the sectional running times for the system are also possible over time. Any changes would be confirmed as part of the access agreement negotiations.

Trains travelling south to Brisbane are travelling in the Up Direction whilst trains travelling north to Cairns are travelling in the Down Direction.

Incident Recovery Time and Management

Historically it is anticipated that on the North Coast Line System a minor incident could result in disruption to services for 6 hours and a major incident for 2 days.

Incident recovery is dependent on the nature, severity and location of each unique incident that may occur on this system.

To enable quick response in case of emergency, latitudes and longitudes of some passing loops, generally direction change, are detailed below :

Location	Latitude	Longitude
Rockhampton	23° 23' S	150° 30' E
Yamba	23° 07' S	150° 22' E
Kunwarara	22° 54' S	150° 08' E
Marlborough	22° 48' S	149° 53' E
Styx	22° 36' S	149° 38' E

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St Lawrence	22° 21' S	149° 31' E
Carmila	21° 54' S	149° 25' E
Yukan	21° 31' S	149° 13' E
Sarina	21° 25' S	149° 13' E
Mackay	21° 08' S	149° 10' E
Erakala	21° 07' S	149° 07' E
Mount Ossa	20° 57' S	148° 50' E
Bloomsbury	20° 42' S	148° 35' E
Proserpine	20° 24' S	148° 34' E
Bowen	20° 00' S	148° 13' E
Merinda	20° 00' S	148° 10' E
Gumlu	19° 52' S	147° 41' E
Home Hill	19° 39' S	147° 24' E
Pioneer	19° 32' S	147° 19' E
Giru	19° 30' S	147° 06' E
Stuart	19° 20' S	146° 50' E
Townsville	19° 15' S	146° 48' E
Purono	19° 11' S	146° 34' E
Mutarnee	18° 57' S	146° 17' E
Ingham	18° 38' S	146° 09' E
Cardwell	18° 16' S	146° 01' E
Bilyana	18° 07' S	145° 54' E
Tully	17° 55' S	145° 56' E
Innisfail	17° 31' S	146° 01' E
Deeral	17° 13' S	145° 55' E
Gordonvale	17° 05' S	145° 47' E
Cairns	16° 55' S	145° 46' E
Yeppoon	23° 07' S	150° 44' E
Marian	21° 10' S	148° 59' E

Rail / Road Interfaces

Operators on this section of the North Coast Line System will encounter 725 Rail / Road Interfaces and 46 Rail / Rail Interfaces (see Appendix C for details) categorised as follows:-

Public (Active with Flashing Light/Boom Gate Protection)	-	173
Public (with Passive Protection - Signs)	-	268
Occupation (Private Access)	-	284
Drawbridge / Tramway	-	46

Rail Operations and the Environment

All railway operators are required to comply with all relevant State, Federal and Local

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Legislation and Laws, current at the time, relating to the management and protection of the Environment.

Queensland Rail currently has a number of licences and/or approvals for activities undertaken at either Queensland Rail facilities or on the Queensland Rail corridor. Queensland Rail's licences and approvals fall under two main areas:

1. Fixed Locations;

Queensland Rail has a number of licences for activities managed by its operational Business Groups in particular locations, such as refuelling locations.

2. Itinerant or Varied Locations;

Queensland Rail also has a number of licences for activities that occur at more than one location, such as maintenance activities.

Railway operators will need to ascertain with the Department of Environment and Heritage Protection or Other Regulatory Body their responsibilities in regard to obtaining an Environmental Authority(ies) for the type of operation proposed.

Copies of all Environmental Authorities administered by the Department within Queensland are available upon request from the Department. The Department of Environment and Heritage Protection contact details can be found via the web at

<https://www.ehp.qld.gov.au/>

Environmental Noise

The Environmental Protection (Noise) Policy (EPP Noise) recognises a railway as a beneficial asset, which is necessary for the community's environmental, social and economic well-being. The Environmental Protection (Noise) Policy is available from the Office of the Queensland Parliamentary Council website at

<http://www.legislation.qld.gov.au/OQPChome.htm>

The EPP Noise nominates "planning levels" for railway noise which may be used as a guide in deciding a reasonable noise level for the activity. The EPP Noise recognises, however, those levels may not be appropriate for an existing railway. It envisages that it may be reasonable to apply the levels only in the long term to allow time to progressively reduce any significantly adverse effects on the environmental values from its operation. The long term planning levels are:

L_{Aeq} (24 hour)	65dBA
L_{Amax}	87dBA

They are to be assessed one (1) metre in front of the most exposed part of the building facade of

an affected noise sensitive place.

Noise Management

While noise from the operation of a railway is exempt from environmental nuisance provisions under the Queensland Environment Protection Act 1994, Queensland Rail strives to manage noise associated with both its rail operations and network wherever reasonable and practical.

As the rail manager, Queensland Rail works closely with customers regarding environmental issues, and provides feedback to Rail Operators to allow them to investigate and address as applicable, noise related issues that may be associated with their locomotives and wagons.

There are various sources of noise from a railway and to aid efficient and effective noise reduction, a range of noise management measures are utilised by Queensland Rail. These are detailed at:

<http://www.queenslandrail.com.au/inthecommunity/environment/noisemanagement>

Wheel Squeal & Flanging

Wheel Squeal is caused by friction forces between the top of rail and wheel interface. Whereas, flanging noise is predominantly caused by friction forces between the side of rail and wheel interface. Continuous or sustained wheel squeal produced primarily on the low rail side, is distinct from discontinuous “flanging noise” that is produced on the high rail side. Continuous wheel squeal is of a high level, and Queensland Rail’s experience is that it may cause significant community reaction, while flanging noise is of a lower level and is more accepted by the community.

Generally, tighter radius curves (i.e. under 300 metre radius) when associated with a number of rollingstock factors that promote wheel squeal, may result in squeal being produced. Rollingstock factors that may promote wheel squeal include:

- Higher wheel hardness
- Stiff primary suspensions
- High centre plate friction
- Worn wheel treads
- Misaligned axles
- Unmatched wheel tread diameters, and
- Incorrectly adjusted sidebearers

Noise Complaints

Queensland Rail is corporately committed to act towards its neighbours in a considerable and reasonable manner. This good neighbour commitment assumes a reasonable degree of tolerance from neighbours and a commitment by Queensland Rail to take action where appropriate.

Where Queensland Rail receives complaints about noise from railway activities for which Queensland Rail may be responsible, Queensland Rail responds to those complaints and maintains

records of those complaints in accordance with its Environmental Management System (EMS).

Where available, generic data will be supplied on request to a third party operator who is proposing operations within a defined network. That data will indicate those areas where Queensland Rail has received prior complaints relating to its train operations. It will be made available when a third party operator is undertaking the development of its Environmental Investigation and Risk Management Report as part of its Access Agreement conditions.

Third Party Requirements

Any railway operator obtaining access to Queensland Rail's Network shall be required to commission an environmental investigation of the proposed operations. This investigation will be conducted by a suitably qualified person, reasonably acceptable to both parties.

In response to the findings of such an investigation, the operator shall produce an Environmental Investigation and Risk Management Report that identifies the risks of Environmental Harm associated with the operation and provides proposed controls to address the risks. This shall be reviewed by, and agreed with, Queensland Rail.

In addition, the operator shall have in place an EMS, which, amongst other things, has regard for the issues, risk and control measures identified in the Environmental Investigation and Risk Management Report. Further details on requirements for environmental issues can be found in Queensland Rail's Access Undertaking.

Queensland Rail has determined that it holds no EMS documentation that, without disclosure to a third party operator, would either:

- Compromise or restrict a third party's operations or increase or place at risk the environmental performance of the third party operator or itself, and
- Limit or restrict the abilities of a third party operator to develop such documentation that would not be reasonably expected of the operator to develop on its own behalf, commensurate with the size and subsequent environmental risks of the proposed operations and the organisational resources available to it, to undertake such operations.

Any EMS documentation (wholly or partially) identified as specifically relating to the control of corridor infrastructure (below rail) environmental issues, will be made available to the operator to assist in formulating appropriate and consistent operational (above rail) controls within their Environmental Investigation and Risk Management Report and EMS.

Maximum Train Length

The maximum length of trains is determined by:

- requirements for crossing/passing other trains
- requirements for braking performance of the train
- capacity of the route
- drawgear capacity
- train handling
- requirements for road/pedestrian access across the track

Where it is necessary for a train to cross, pass or be passed by another train, the maximum train length allowable shall be such that the comparison train length (including allowance for stretching and train handling) is not longer than the crossing loop length.

Variations of train length for a particular train configuration is possible and would need to be negotiated as part of access agreement negotiations.

Rollingstock Braking Rate

The signalling system and flashing light protection at rail / road interfaces has been designed to cater for the variety of trains that currently use this system.

Signal design parameters and train braking characteristics will be compared during the development of the Interface Risk Management Plan.

Future Infrastructure Improvements

Capacity Enhancements

Queensland Rail welcomes opportunities to work with customers with a view to transporting additional tonnages on this System.

We encourage Rail Operators, mining companies and/or processors to engage with Queensland Rail at the earliest possible opportunity. This will allow sufficient time to work through detailed capacity analysis and to determine the network upgrades necessary and negotiate appropriate commercial arrangements.

Capacity enhancements will continue to be delivered for future projects provided that contracted tonnages:

- Are sufficient to justify the necessary capital investment on commercial terms; and
- Adequate notice is given from the time of contracting capacity to deliver the required enhancements.

Infrastructure Management and Access

APPENDIX B - SCHEMATIC LAYOUT is colour coded to indicate Management of Infrastructure and Access.

Third party access to non-Queensland Rail managed infrastructure is by commercial arrangement with the relevant party.

The initial point of contact for Queensland Rail managed below rail assets is:

General Manager Access Revenue

Level 9 | 305 Edward Street
Brisbane Qld 4001

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Telephone 61 07 3072 1145

Facsimile 61 07 3072 8248

Email: aarf@qr.com.au

APPENDIX A

Definitions (Statewide)

Access Agreement

Access Agreement means an agreement between Queensland Rail and an Access Holder for the provision of Access.

Access Undertaking

A document approved by the Queensland Competition Authority (QCA) in accordance with the QCA Act 1997 (Q) that sets out principles for negotiating access to Queensland Rail's declared services.

Accreditation

Accreditation in accordance with part 4, Chapter 6 of the Transport Infrastructure Act 1994 (Qld) and "Accredited" has a similar meaning.

ATP (Automatic Train Protection)

Automatic Train Protection is a computer controlled system designed to make sure the train

- does not exceed the current speed limit
- does not exceed the limit of authority generated by the interlocking (and usually indicated by a signal at STOP)
- does not make unreasonable train movements during shunting, when stationary, or at startup

AWS (Automatic Warning System)

Automatic Warning System is designed to

- provide an in-cab visible and audible indication of the aspect displayed in the next signal
- prompt and warn the train driver of a RESTRICTED signal aspect displayed in the next signal
- stop the train if the driver fails to acknowledge the AWS alarm of a RESTRICTED signal aspect

Axle Counters

At some locations in Remote Controlled Signalling (RCS) Territory an axle counter system has been provided to detect occupancy of a section of track.

An axle counter at each end of a section determines whether an axle is entering or leaving the section and counts the number of axles passing the counter in each direction. By keeping an accurate count of axles into the section, then the number of axles out of the section, the system can determine if the section is occupied or not.

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Block Train

A train consisting entirely of similar classes of wagons of axle loads over 12.2 tonnes marshalled together for a certain class of traffic. The definition is also extended to cover trains in which 12 or more such wagons loaded to more than 12.2 tonnes gross per axle are included within a length of 315 metres or less of the train.

Crossing Loop Length

The maximum length in metres of the train which can be accommodated in the loop to allow normal operation of the signalling systems for crossing or passing movements.

Daily Train Plan (DTP)

Collectively, the scheduled times for all Train Services operating on Queensland Rail's Rail Infrastructure and any Planned Possession on a particular day.

Declared Services

Services declared as available for access by third party operators in accordance with the QCA Act 1997 (Q).

Declared Infrastructure

Infrastructure declared as available for access by third party operators in accordance with the QCA Act 1997 (Q).

Design Neutral Temperature

The rail temperature at which the track is designed to be stress free as defined in Queensland Rail's Civil Engineering Publication #26 "Rail Stressing Manual".

Direct Traffic Control (DTC)

Direct Traffic Control (DTC) is an absolute block safeworking system used to control the movement of trains in non-signalled territory.

Central to DTC is an on-board DTC computer which displays authorities stored in its database. The relevant authority is activated by the train crew following an exchange of codes between the crew and the controller. Codes are exchanged verbally using the train control radio.

The procedures governing the operation of DTC are detailed in Queensland Rail's Standard MD-10-113 "Direct Traffic Control Manual".

Dragging Equipment Detectors (DED)

A mechanism positioned on sections of track to detect any dragging equipment on train.

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Dragging Equipment Detectors Alarm (DED Alarm)

Part of the Queensland Rail System which advises the Train Controller either by a computer prompt message that a D.E.D. has been activated and the train driver by a recorded voice message.

Electromagnetic Compatibility (EMC)

The ability of an equipment or system to function satisfactorily in its electromagnetic environment without introducing intolerable electromagnetic disturbances to anything in that environment.

Electric Train Staff

A 'token' system of train working between Interlockings on single lines in non track- circuited areas, where release of a token is controlled by electrically connected and interlocked instruments.

EPP (Noise)

Environmental Protection (Noise) Policy 1997; Subordinate Legislation to the Queensland Environmental Protection Act 1994.

Force Majeure Event

Means any cause, event or circumstance, or combination of causes, events or circumstances, which is beyond the reasonable control of the Party affected thereby and which by the exercise of due diligence such Party is not reasonably able to prevent or overcome, including but not limited to, results of abnormal weather conditions, act of God, breakdown of any facilities or machinery or unavailability of essential equipment, strikes or other industrial dispute.

Hot Wheel & Bearing Detectors (HWD/HBD)

Heat sensors located at strategic locations on the system that identify abnormal temperatures in wheels and wheel bearings as the train passes over, transmits a signal to the train control panel that necessitates an inspection of the suspect wagon and remedial action

Line Code

Line Code, a unique alpha-numeric identifier applied to a section of track on Queensland Rail's network and usually run from junction point to junction point. Each numeric identifier is unique and can be further rolled up into Corridors using the alpha identifier.

LWR

Long welded rail. Rail that has mechanical rail joints spaced at intervals between 110m and 220m.

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LSC

Line Section Code, a unique alpha-numeric identifier applied to a section of Queensland Rail's network.

Master Train Plan (MTP)

Collectively, the scheduled times as advised by Queensland Rail from time to time for all Train Services operating on Queensland Rail's Rail Infrastructure where such scheduled times remain unchanged from week to week, and any Planned Possessions.

Nominal Rail Size

Rail sizes 20, 31 and 41 kg/m are all nominal rail sizes used to group together a range of rail types and sizes originally designated in the imperial unit "lb/yd". The term "nominal" is used in recognition of the variation in the dimensions, mass and engineering properties of the rails in this category.

Ordinary Staff and Ticket Working

A token based system of safeworking where the movement of trains on bi-directional single lines is on possession of a staff token or ticket. Each section of single line has a unique token.

Staff & Ticket

The Staff and Ticket System allows for the movement of trains over a bidirectional track.

The Staff and Ticket System operates (in accordance with Queensland Rail's Standard MD-10-114) on the principle of absolute block working, which provides that only one train will be authorised to be on any one section at any one time.

Railway Operator

A person who has, or is seeking, Access from Queensland Rail to operate Train Services on the Rail Infrastructure and who is, or who will become, Accredited in respect of those Train Services.

Remote Controlled Signalling (RCS)

A system of Safeworking where train movements are governed by aspects displayed in Colour Light Signals which are controlled from a remote location and by the passage of trains. Some colour light signals and points may be released by the Train Controller to be operated from a local area by using:

- a local control panel;
- an electrically released shunting frame;
- a zone released shunting system, or
- emergency push buttons.

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Railway Operators trains are expected to meet existing signalling standards to ensure track circuits and other signalling equipment operate safely and effectively - in particular Queensland Rail's Standard MD-10-76 "Principles for the Signalling of Trains" must be complied with.

Rollingstock Authorisation Process

The process for determining and validating rollingstock compliance and registration as detailed in Queensland Rail's Standard MD-10-140 - Rollingstock Validation, Acceptance and Registration

Remote Train Overview Application (RTOA)

A PC based system providing real time operational information, gathering information on train running and rail network status for immediate and continuously updated display and historical analysis.

Being a multi-tier client-server application, different levels of access/security ensure confidentiality of an Operator's train performance statistics.

SN Speed Boards

Speed Normal Boards are speed boards that place the onus on the Driver of a train to travel at speeds considered safe for that section of track being travelled over. These boards are gradually being phased out in accordance with Queensland Rail's Civil Standard MD-10-87 - SPEED BOARDS

Standard Train

The predominant type of train operating on the line/system.

SWR

Short welded rail. Rail that has mechanical rail joints spaced at intervals less than 110m.

Train Authorisation

The process for acceptance of a train configuration whose rollingstock is registered under Queensland Rail's Standard MD-10-140 - Rollingstock Validation, Acceptance and Registration.

Train Length

The total length in metres of a train including the locomotives. For the purposes of comparison with the length of crossing loops, an addition of 1% (1 metre for every 100 metres) shall be allowed to the calculated length of the train to allow for train stretching.

Unit Train

A train composed entirely of the one class and one drawgear classification of rollingstock.

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Universal Traffic Control (UTC)

A PC based train control supervisory system that provides the means to remotely control train movements over a large area and provide management and train users with real time train related information.

ViziRail

A fully integrated scheduling, possession planning, monitoring and reporting tool for managing the Queensland Rail below-rail network.

ViziRail also supports the provision of all QCA and the Department of Transport and Main Roads reporting requirements.

Weather Monitoring System (WMS)

Remote weather monitoring stations providing critical information regarding temperature, rainfall and stream levels.

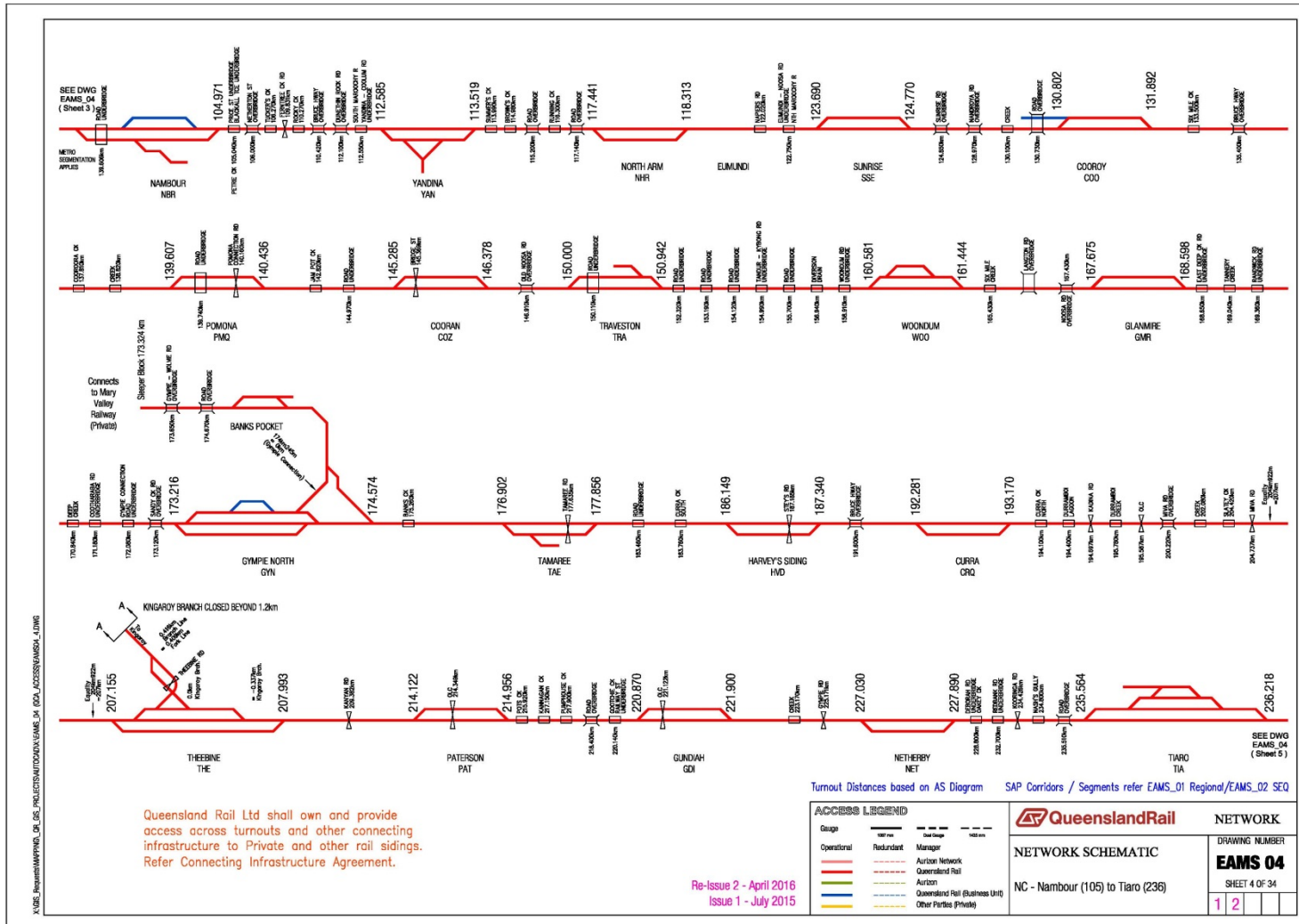
Wheel Impact Load Detector (WILD)

In track monitoring system to identify wheel flats.

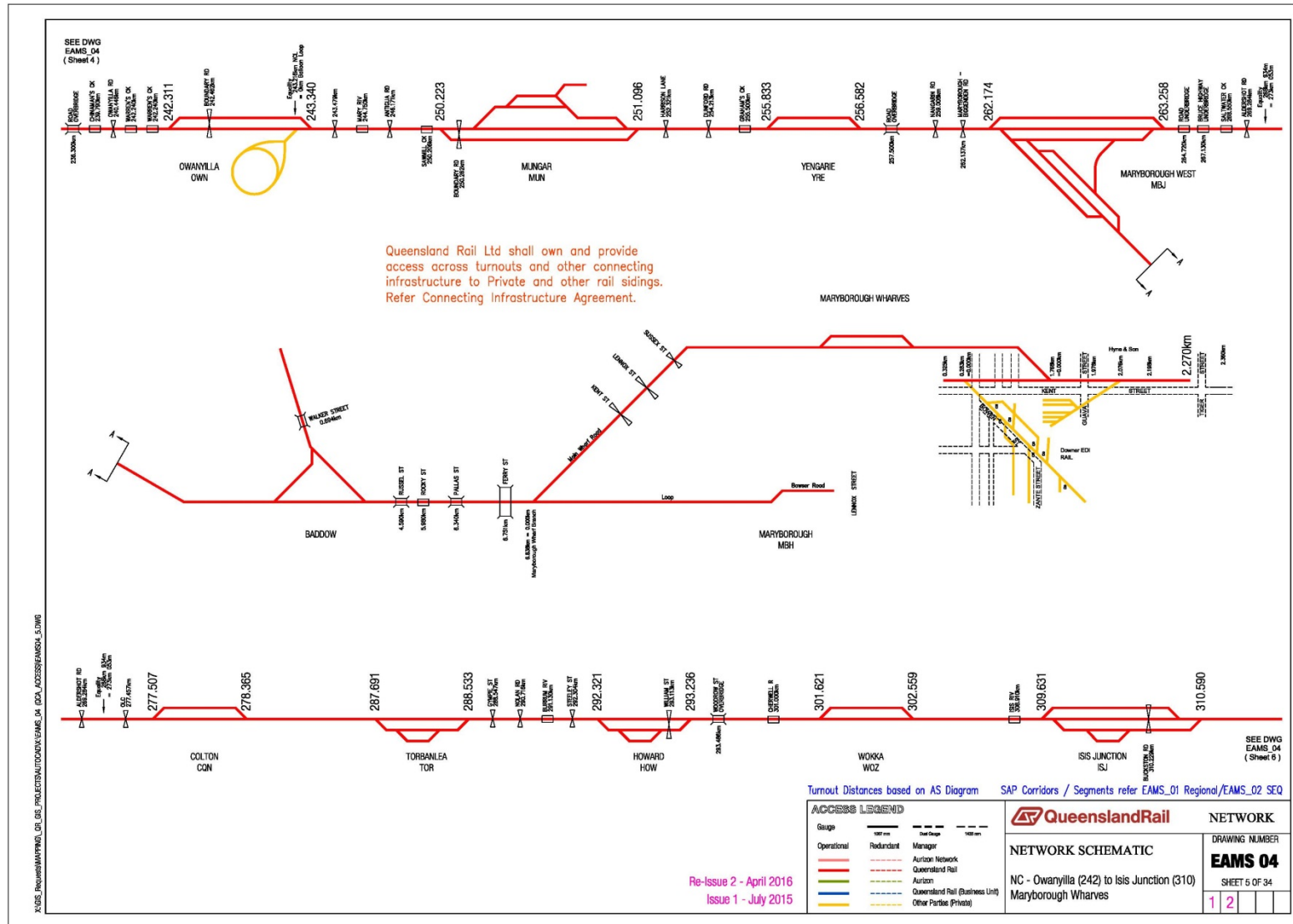
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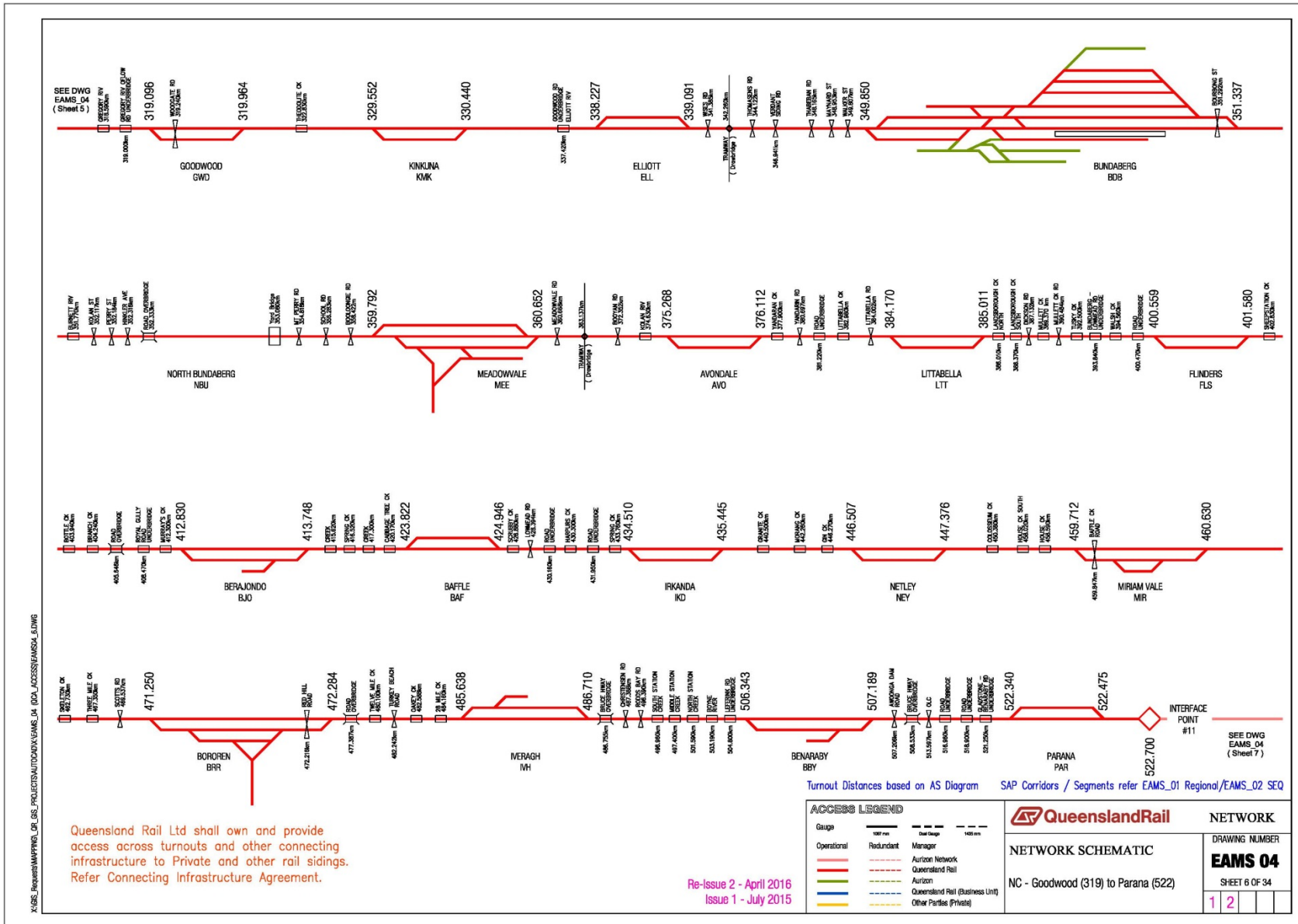
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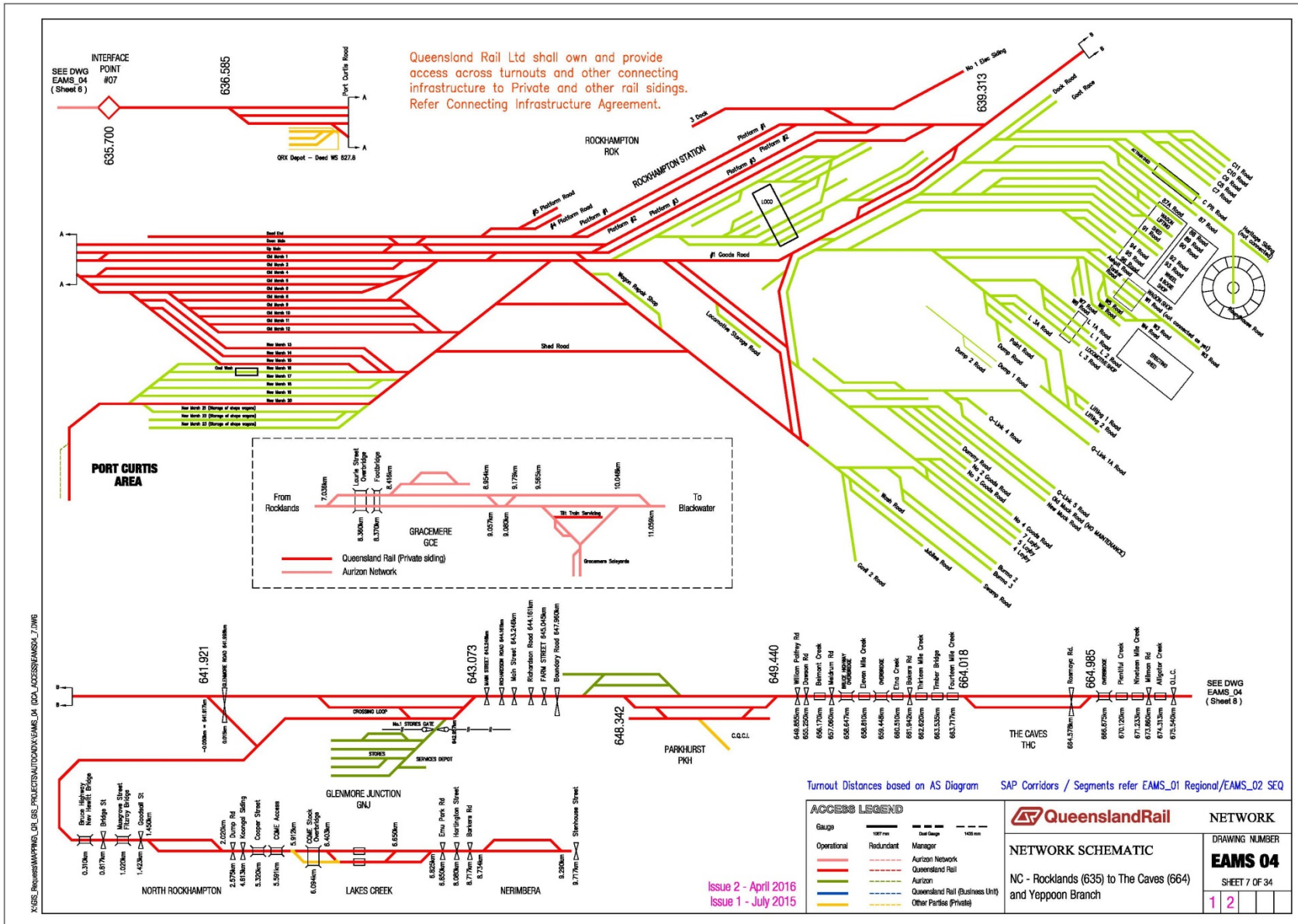


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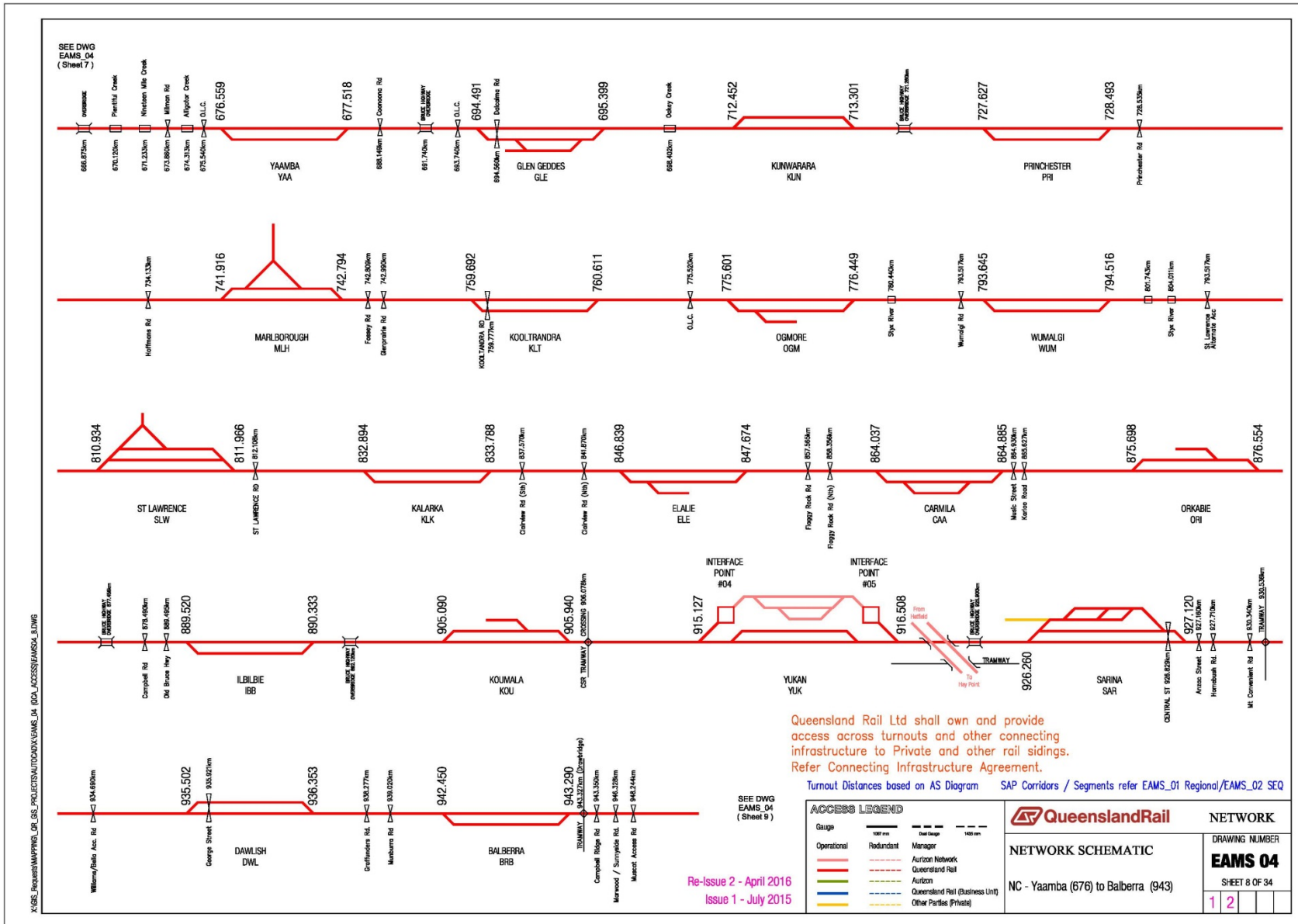


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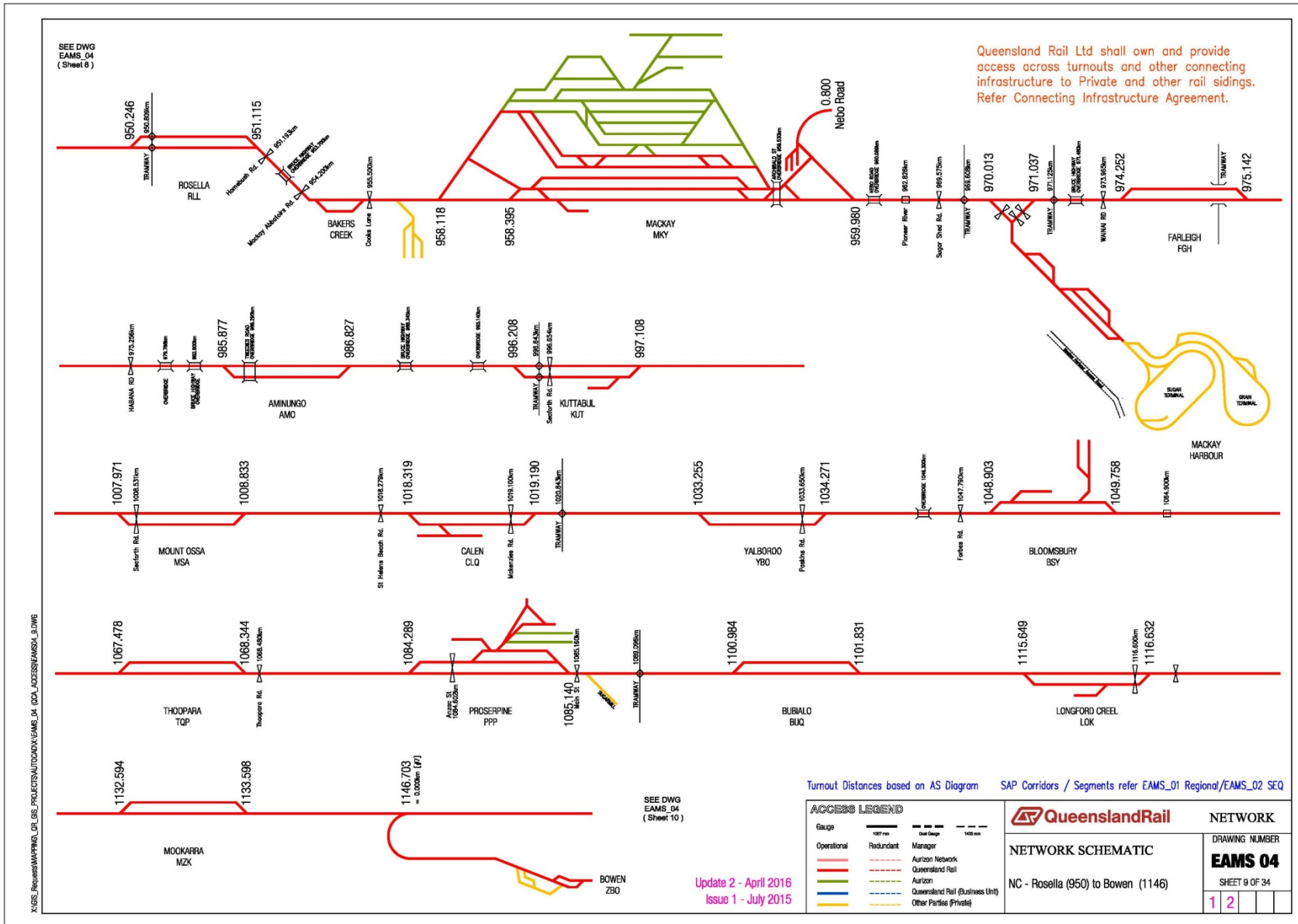
Queensland Rail Ltd shall own and provide access across turnouts and other connecting infrastructure to Private and other rail sidings. Refer Connecting Infrastructure Agreement.



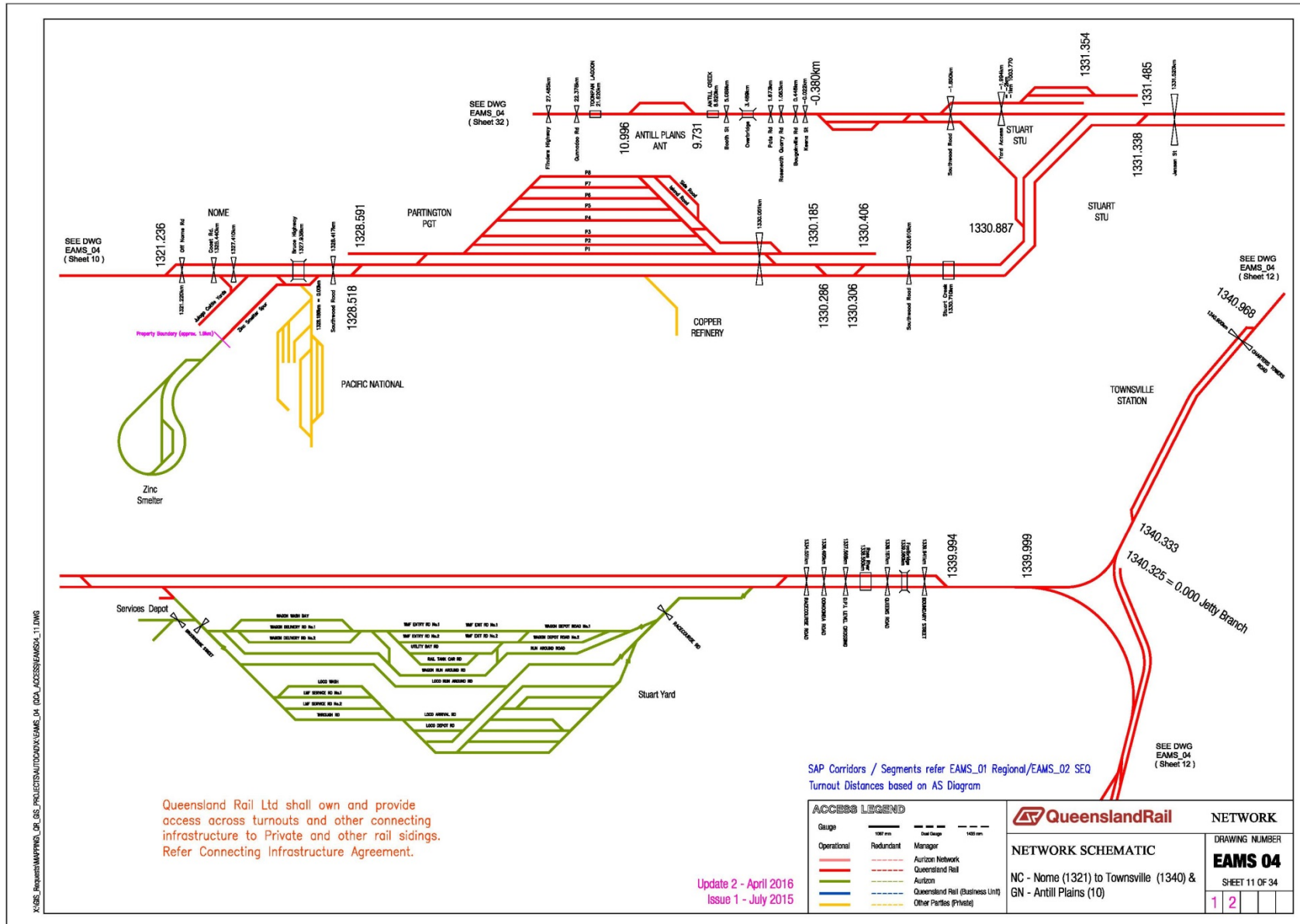
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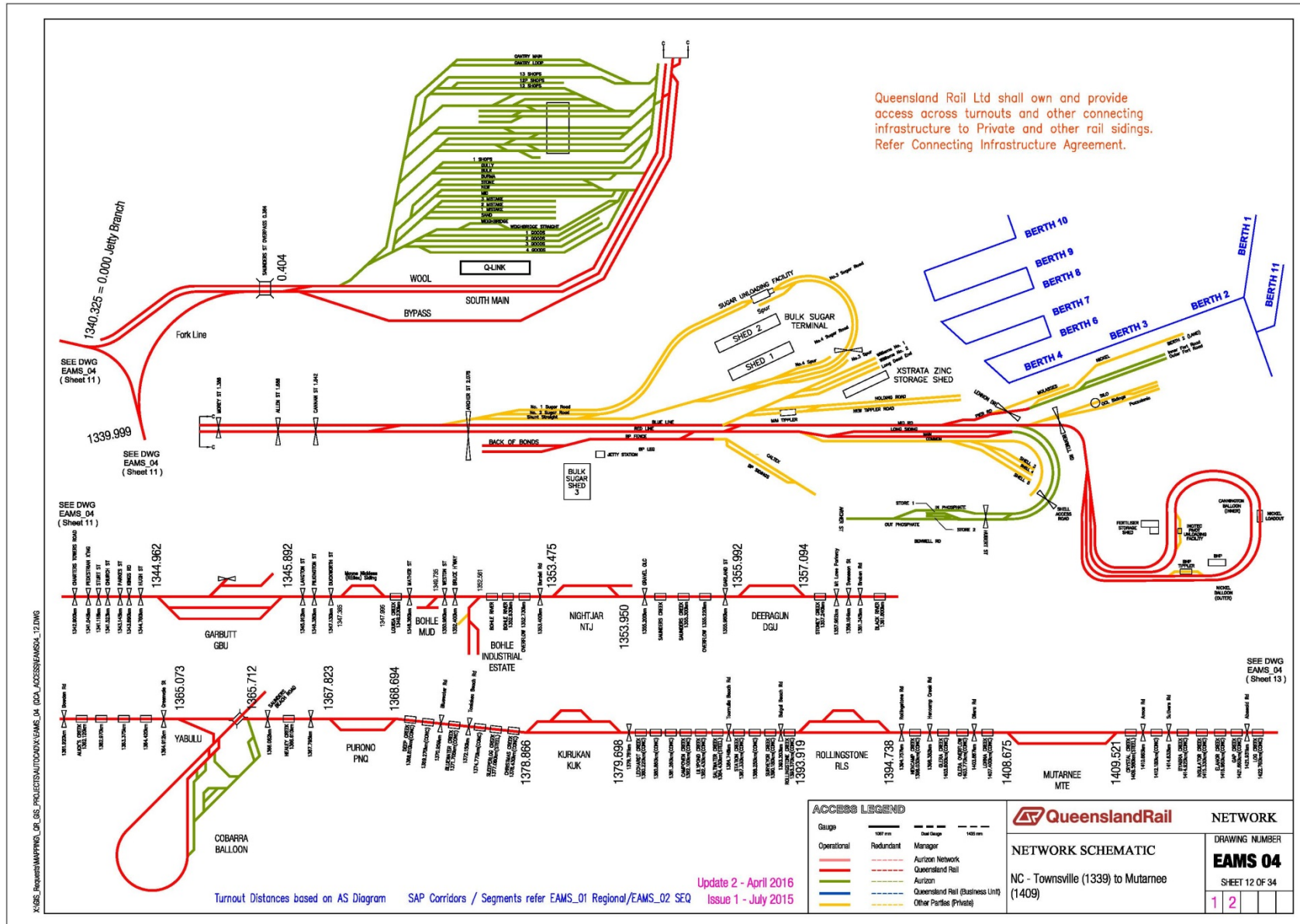
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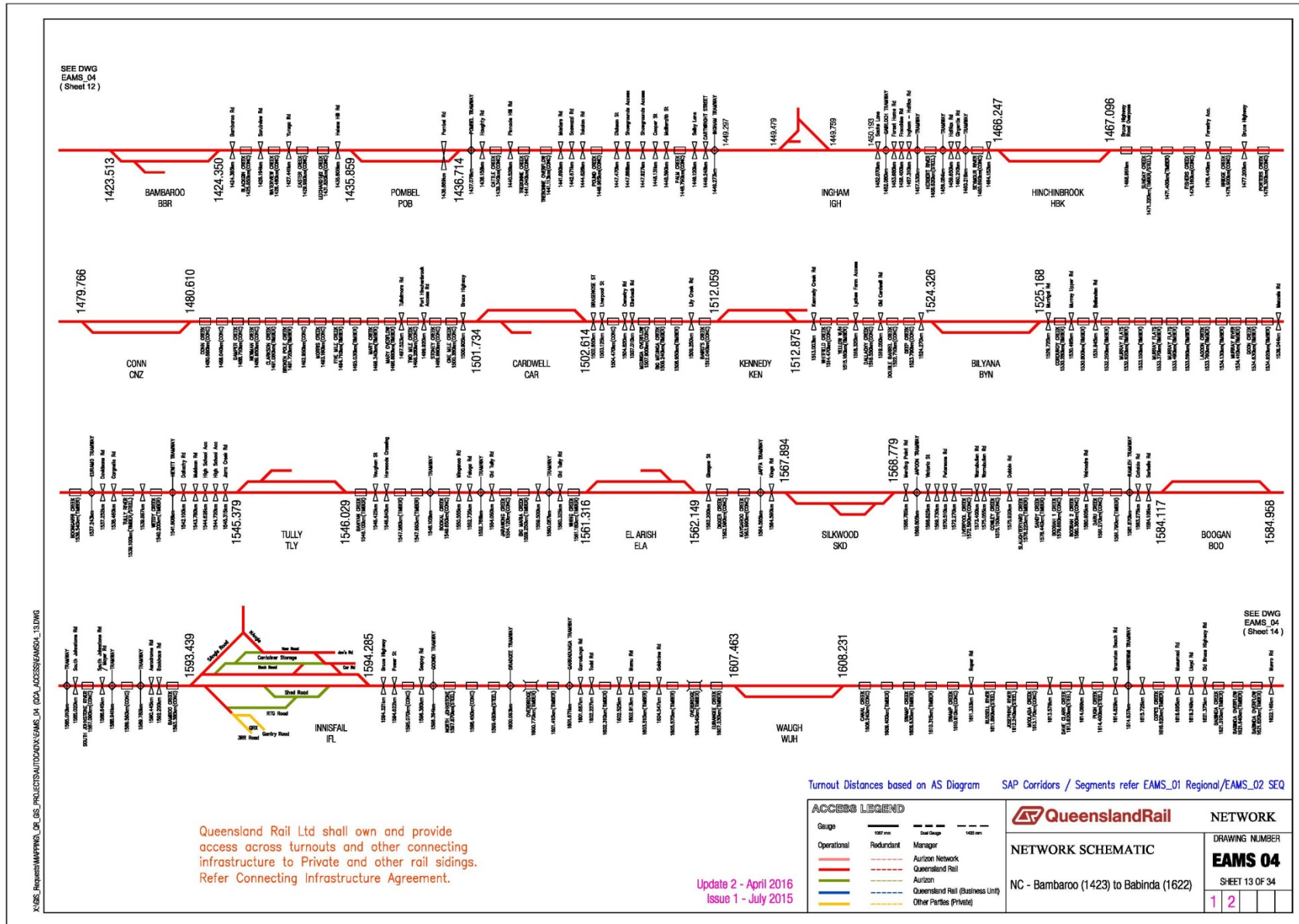
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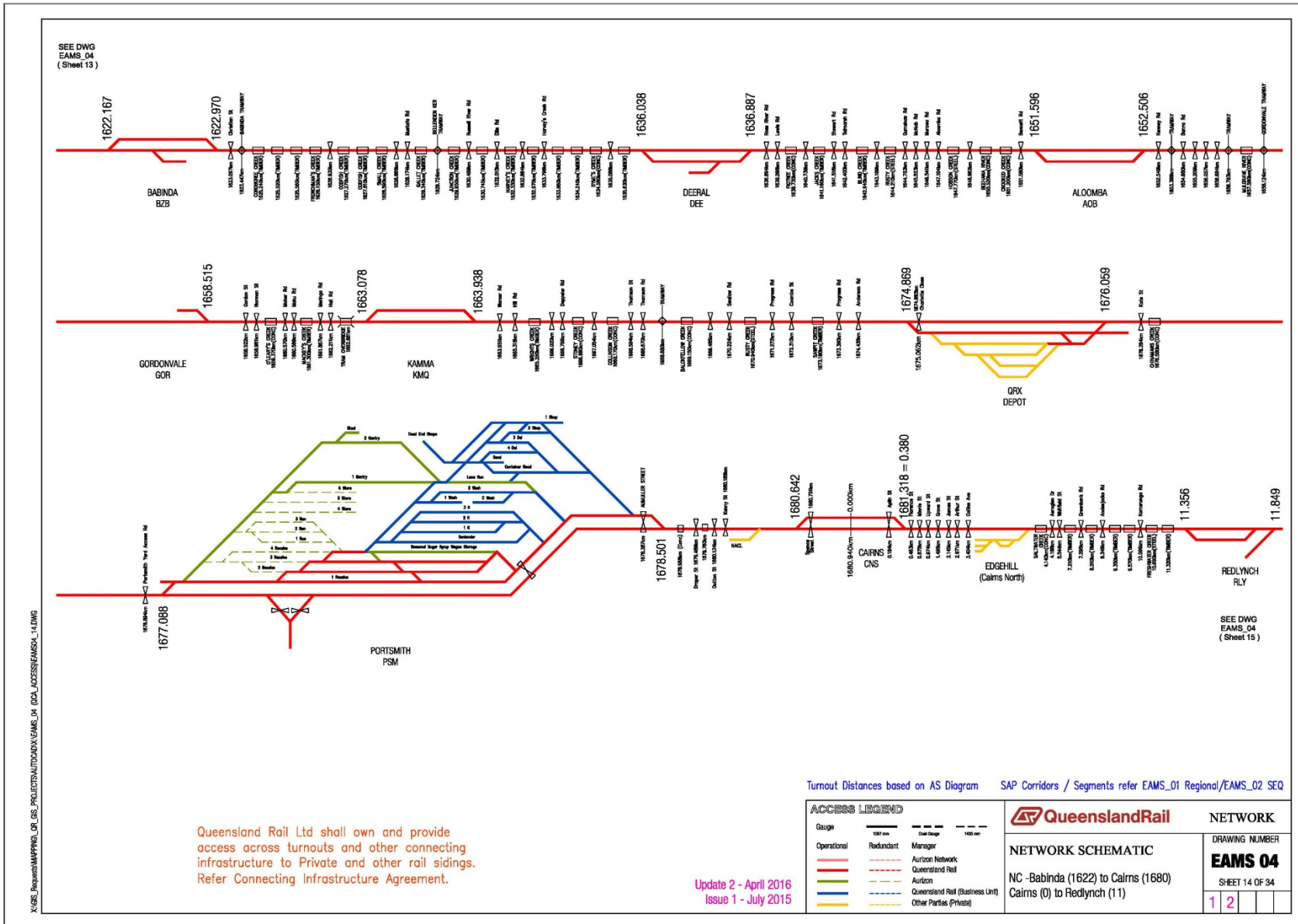
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APPENDIX C

Rail/Road Interface Details

		CROSSINGS BY LINE AND KILOMETRAGE					
SURVEY SECTION		Description of Signage Types					
G	Giveaway	PWB	Pedestrian Warning	T	Triangle		
N	New Signage Type	S	Stop	U	Unknown		
O	Other	SC	School Crossing	X	Crossbuck		
<u>Line Section Code</u>	<u>Km</u>	<u>Road Name</u>	<u>Type</u>	<u>Vehicle Protection</u>	<u>Signs</u>	<u>Open Status</u>	<u>Responsible Authority</u>
BOWEN COKEWORKS							
687	0.130	Don Street (Saltworks Branch)	Public Level	Signs	X G	Open	LGA
687	0.320	Lower Don Road	Public Level	Flashing Lights		Open	LGA
687	0.460	Bridge Road	Public Level	Signs	X G	Open	LGA
687	1.255	Betzels Lane	Public Level	Signs	X G	Open	LGA
687	2.450	Don Street (Bowen Connection Road)	Public Level	Flashing Lights		Open	LGA
ERAKALA FORK							
116	0.125		Occupation	Signs	X G	Open	PRI
116	0.575		Occupation	Signs	X G	Open	PRI
FINCH HATTON BRANCH (Mackay to Marian)							
684	0.280	Jeffcoat Street	Public Level	Signs	X G	Open	LGA
684	0.827	Nebo Road / Bruce Highway	Public Level	Flashing Lights		Open	MRD
684	1.237	Private Access Road	Occupation	Signs	X G	Open	PRI
684	1.394	Private Road	Occupation	Signs	X G	Open	PRI
684	1.746	Walkers Road	Public Level	Signs	X G	Open	LGA
684	2.628	Horse And Jockey Road	Public Level	Flashing Lights		Open	LGA
684	2.920	Sugar Mill Pedestrian Crossing	Pedestrian			Open	PRI
684	2.980		Occupation	Signs	X G	Open	PRI
684	3.150	Racecourse Mill Yard Access Road	Occupation	Signs	X G	Open	PRI
684	3.240		Tramway			Open	
684	3.534	Te-kowal - Foulden Road	Public Level	Signs	X G	Open	LGA
684	4.190	Sextons Road	Public Level	Signs	X G	Open	LGA
684	4.384	Bses Access Road	Occupation	Signs	X G	Open	PRI
684	5.167	Hills Road	Public Level	Signs	X G	Open	LGA
684	5.310	Telstra Exchange Access Road	Occupation	Signs	X G	Open	PRI
684	5.983		Tramway			Open	
684	6.558	Welgands Road	Public Level	Signs	X G	Open	LGA
684	7.300	Mackay / Eungella Road	Public Level	Flashing Lights		Open	MRD
684	7.790		Tramway			Open	
684	8.635	Palms Road	Public Level	Signs	X G	Open	LGA
684	9.933	Private Road	Occupation	Signs	X G	Open	PRI
684	10.264	Branscombe Road	Public Level	Signs	X G	Open	LGA
684	11.979	Old Pleystowe Road	Public Level	Signs	X G	Open	LGA
684	11.986		Tramway			Open	
684	12.990	Pleystowe South Road	Occupation	Signs	X G	Open	PRI

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<u>Line Section Code</u>	<u>Km</u>	<u>Road Name</u>	<u>Type</u>	<u>Vehicle Protection</u>	<u>Signs</u>	<u>Open Status</u>	<u>Responsible Authority</u>
684	13.221		Tramway			Open	
684	13.229		Occupation	Signs	X B	Open	PRI
684	14.327	Pleystowe School Road	Public Level	Signs	X B	Open	LGA
684	15.950	Muscat-borg Access Road	Occupation	Signs	X B	Open	PRI
684	16.499		Tramway			Open	
684	17.333	Wollingford Road	Public Level	Signs	X B	Open	LGA
684	18.481		Occupation	Signs	X	Open	PRI
684	18.950		Occupation	Signs	X B	Open	PRI
684	19.753	Newbury - Mount Vince Road	Public Level	Signs	X B	Open	LGA
684	20.685	Marian Cemetary Access Road	Public Level	Signs	X B	Open	LGA
684	21.000	West's Road	Public Level	Signs	X B	Open	LGA
684	21.396	Kennys Road	Public Level	Signs	X B	Open	LGA
684	22.790		Tramway			Open	
684	22.963	Daly Street	Public Level	Flashing Lights		Open	MRD
MACKAY HARBOUR BRANCH							
682	0.410	Access Road (Southern Leg Of NCL Angle)	Occupation	Signs	X B	Open	PRI
682	0.550	Southern Leg Of Ncl Angle	Occupation	Signs	X B	Open	PRI
682	1.850	Property Access Road	Occupation	Signs	X B	Open	PRI
682	2.350	Glenella / Richmond Road	Public Level	Flashing Lights		Open	LGA
682	3.415	Pioneer Street	Public Level	Flashing Lights		Open	LGA
682	3.725		Occupation	Signs	X B	Open	PRI
682	4.820	Property Access Road	Occupation	Signs	X B	Open	PRI
682	10.435	Spiller Avenue	Public Level	Signs	X B	Open	MHP
682	10.435	Spiller Avenue	Public Level	Signs	X B	Open	MHP
682	10.850	Harbour Road	Public Level	Signs	X G	Open	MHP
682	10.950	Graeme Heggie Street	Public Level	Signs	X B G	Open	MHP
682	11.280	Harbour Road	Public Level	Signs	X G	Open	MHP
NORTH COAST LINE							
403	639.342	Stanley Street	Public Level	Signs	X B	Open	LGA
403	639.574	Derby Street	Public Level	Signs	X B	Open	LGA
403	639.805	William Street	Public Level	Signs	X B	Open	LGA
403	640.040	Denham Street	Public Level	Signs	X B	Open	LGA
403	640.270	Fitzroy Street	Public Level	Half Boomgates		Open	MRD
403	640.499	Archer Street	Public Level	Signs	X B	Open	LGA
403	640.730	Cambridge Street	Public Level	Signs	X B	Open	LGA
403	640.963	Albert Street (Bruce Highway)	Public Level	Half Boomgates		Open	MRD
403	641.205	North Street	Public Level	Signs	X B	Open	LGA
403	641.441	Bolsover Street	Public Level	Half Boomgates		Open	LGA
831	641.998	Glenmore Road	Public Level	Half Boomgates		Open	LGA
831	643.246	Main Street	Public Level	Half Boomgates		Open	LGA
831	643.588	Taylor Street Pedestrian Access	Pedestrian			Open	LGA
831	644.161	Richardson Road	Public Level	Half Boomgates		Open	LGA
831	645.060	Farm Street	Public Level	Half Boomgates		Open	LGA
831	647.960	Boundary Road	Public Level	Flashing Lights		Open	LGA
832	649.855	William Palfrey Road	Public Level	Flashing Lights		Open	LGA

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<u>Line Section Code</u>	<u>Km</u>	<u>Road Name</u>	<u>Type</u>	<u>Vehicle Protection</u>	<u>Signs</u>	<u>Open Status</u>	<u>Responsible Authority</u>
832	655.250	Dawson Road	Public Level	Flashing Lights		Open	LGA
832	657.060	Meldrum Road	Public Level	Flashing Lights		Open	LGA
832	661.942	Bakers Road	Public Level	Signs	X 0	Open	LGA
832	664.576	Rossmoya Road	Public Level	Half Boomgates		Open	LGA
832	673.861	Milman Road	Public Level	Flashing Lights		Open	LGA
832	675.513	Fairy Bower Road	Public Level	Signs	X 0	Open	LGA
832	676.878	Yaamba Sliding Access Road	Public Level	Signs	X 0	Open	LGA
832	682.722	Access Road	Public Level	Signs	X 0	Open	LGA
832	688.149	Canoono Road	Public Level	Signs	X 0	Open	LGA
832	692.567	Property Access Road	Occupation	Signs	X 0	Open	PRI
832	693.738	Road at Canoono (on stock route)	Public Level	Signs	X 0	Open	LGA
832	694.465	Dalcalmah Road	Public Level	Flashing Lights		Open	LGA
833	697.170	Property Access Road	Occupation	Signs	X 0	Open	PRI
833	705.122	Access Road	Occupation	Signs	X 0	Open	PRI
833	706.460	Property Access Road	Occupation	Signs	X 0	Open	PRI
833	709.662	Access Road	Occupation	Signs	X 0	Open	PRI
833	713.139	Brett Road	Public Level	Signs	X 0	Open	LGA
833	728.535	Princhester Sliding Access Road	Public Level	Flashing Lights		Open	LGA
833	734.133	Hoffmans Road	Public Level	Signs	X 0	Open	LGA
833	742.800	Fossey Drive	Public Level	Half Boomgates	PWB	Open	LGA
833	742.990	Glenprairie Road	Public Level	Signs	X 0	Open	LGA
833	744.844	Landsbergs Access Road	Occupation	Signs	X 0	Open	PRI
833	748.246	Pine Mountain Road	Public Level	Signs	X 0	Open	LGA
833	759.777	Kooltandra Road	Public Level	Flashing Lights		Open	LGA
833	763.401	Strathmuir Station Access Road	Occupation	Signs	X 0	Open	PRI
833	771.661	Bowman Station Access Road	Public Level	Signs	X 0	Open	LGA
833	773.459	Oakdean Station Access Road	Occupation	Signs	X 0	Open	PRI
833	775.520	Brigalow Street	Public Level	Flashing Lights	PWB	Open	LGA
833	777.437	Hares Station Access Road	Occupation	Signs	X 0	Open	PRI
833	778.383	Hartwig's Station Access Road	Occupation	Signs	X 0	Open	PRI
833	779.844	Styx Road	Public Level	Signs	X 0	Open	LGA
833	781.849	Road	Public Level	Signs	X 0	Open	LGA
833	786.381	Access Road	Occupation	Signs	X 0	Open	PRI
833	791.038		Occupation	Signs	X 0	Open	PRI
833	792.628	Access Road	Occupation	Signs	X 0	Open	PRI
833	793.517	Wumalgi East Road	Public Level	Flashing Lights	X	Open	LGA
833	806.753	Frank's Access Road	Occupation	Signs	X 0	Open	PRI
833	810.892	St Lawrence Emergency Access Road	Public Level	Signs	X 0	Open	LGA
680	812.106	St Lawrence Road	Public Level	Flashing Lights		Open	MRD
680	815.332	Private Access Road	Occupation	Signs	X 0	Open	PRI
680	821.152	Schneiders Road	Public Level	Signs	X 0	Open	LGA
680	824.331	L.G. And A.C. Hardwick Access Road	Occupation	Signs	X 0	Open	PRI
680	828.906	Access Road	Occupation	Signs	X 0	Open	PRI
680	833.568	Kalarka Station Crossing	Occupation	Signs	X 0	Open	PRI
680	837.574	Colonial Drive (Clairview Road South)	Public Level	Flashing Lights		Open	LGA
680	841.878	Colonial Drive (Clairview Road North)	Public Level	Flashing Lights		Open	LGA
680	847.701	Access Road	Occupation	Signs	X 0	Open	PRI
680	852.593	Blue Water Huts Access Road	Occupation	Signs	X 0	Open	PRI
680	857.565	Flaggy Rock Road (Douglas Road)	Public Level	Flashing Lights		Open	LGA

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680	858.356	North Flaggy Rock Road	Public Level	Signs	X Ⓟ	Open	LGA
680	864.929	Music Street - Carmila Beach Road	Public Level	Flashing Lights	X	Open	LGA
680	865.656	Karloo Road	Public Level	Flashing Lights	X	Open	LGA
680	868.618	Tinerta Road	Public Level	Signs	X Ⓟ	Open	LGA
680	874.009	Road	Public Level	Signs	X Ⓟ	Open	LGA
680	874.621	Smiths Stock Crossing	Occupation	Signs	X Ⓟ	Open	PRI
680	876.625	Fredrick Hoare Drive	Public Level	Flashing Lights		Open	LGA
680	878.492	Campbells - Crebers Road	Public Level	Flashing Lights	X	Open	LGA
680	881.518	Gavigilas Road (Gunters Hill Access Road)	Occupation	Signs	X Ⓟ	Open	PRI
680	887.585	Bella Access, Koota Road	Occupation	Signs	X Ⓟ	Open	PRI
680	889.507	Old Bruce Highway	Public Level	Flashing Lights	X	Open	LGA
680	896.640	Mt. Christian Station Road	Public Level	Signs	X Ⓟ	Open	LGA
680	900.007	Prices Road	Public Level	Signs	X Ⓟ	Open	LGA
680	900.966	Thomsets Road	Occupation	Signs	X Ⓟ	Open	PRI
680	902.024	Access Road	Occupation	Signs	X Ⓟ	Open	PRI
680	902.181	Archies Road	Public Level	Signs	X Ⓟ	Open	LGA
680	903.348	Berardi Access Road	Occupation	Signs	X Ⓟ	Open	PRI
680	904.563	Tedlands Road	Public Level	Signs	X Ⓟ	Open	LGA
680	906.078		Tramway			Open	LGA
680	906.695	Landing Road	Public Level	Signs	X Ⓟ	Open	LGA
680	907.784	B & M Bella Access Road	Occupation	Signs	X Ⓟ	Open	PRI
680	908.938	Lolama Road	Public Level	Signs	X Ⓟ	Open	LGA
680	910.356	South Inneston Road	Public Level	Signs	X Ⓟ	Open	LGA
680	912.710	North Inneston Road	Public Level	Signs	X Ⓟ	Open	LGA
408	916.051	Borgs Access Road / QR Maintenance QR Road		Signs	X Ⓟ	Open	QR
408	917.017	The Glen Access Road	Occupation	Signs	X Ⓟ	Open	PRI
408	921.405	Oonoole Road	Public Level	Flashing Lights		Open	LGA
408	922.488	Bailey's Access Road	Occupation	Signs	X Ⓟ	Open	PRI
408	923.694	Keating's Access Road	Occupation	Signs	X Ⓟ	Open	PRI
408	924.793	Armstrong Beach Road	Public Level	Half Boomgates	X	Open	LGA
408	926.257	Mill Access Road	Occupation	Flashing Lights	X	Open	PRI
408	926.828	Central Street	Public Level	Half Boomgates	X	Open	MRD
409	927.142	Anzac Street	Public Level	Half Boomgates		Open	MRD
409	928.387	Mill Yard Access Road	Occupation	Signs	X Ⓟ	Open	PRI
409	929.703	Garina / Homebush Road	Public Level	Flashing Lights	X	Open	MRD
409	930.339	Mt Convenient Road	Public Level	Signs	X Ⓟ	Open	LGA
409	930.536		Tramway			Open	
409	931.270	Plaths Road	Public Level	Signs	X Ⓟ	Open	LGA
409	934.691	McMahon Road (Williams / Bella Access Road)	Public Level	Signs	X Ⓟ	Open	LGA
409	935.898	Dawlish Road (George Street)	Public Level	Half Boomgates	X	Open	LGA
409	938.276	Boyds Road	Public Level	Signs	X Ⓟ	Open	LGA
409	939.012	Munburna Road	Public Level	Flashing Lights		Open	LGA
409	939.796	Bartolo Access Road	Occupation	Signs	X Ⓟ	Open	PRI
409	943.327	Campbells Ridge Road	Drawbridge			Open	PRI
409	943.349	Balbera Road	Public Level	Flashing Lights		Open	LGA
409	944.119	Bowles Access Road	Public Level	Signs	X Ⓟ	Open	LGA
409	946.249	Marwood-Sunnyside Road	Public Level	Flashing Lights		Open	LGA

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	409	948.244	Muscat Access Road	Occupation	Signs	X S	Open	PRI
	409	948.949	Cheiona - Sandford Road	Public Level	Flashing Lights		Open	LGA
	409	949.248	Farm Access Road	Occupation	Signs	X S	Open	PRI
	409	950.788	Hamilton Access Road	Occupation	Signs	X S	Open	PRI
	409	950.809	Tramway To Racecourse Mill	Tramway			Open	
	409	951.209	Homebush Road	Public Level	Flashing Lights		Open	MRD
	409	953.212	Gorman Street	Public Level	Flashing Lights		Open	LGA
	409	954.254	Abattoirs Access Road.	Occupation	Flashing Lights		Open	PRI
	409	955.497	Cooks Lane	Public Level	Signs	X S	Open	LGA
	409	957.382	Farrellys Lane	Public Level	Flashing Lights		Open	LGA
	409	958.146	Connors Road (On Private Sliding)	Public Level	Flashing Lights		Open	LGA
	409	958.840	Diesel Fuel Access Road	QR			Open	QR
	409	959.034	Station Yard Access	QR			Open	QR
	836	963.926	Farm Access Road	Occupation	Signs	X S	Open	PRI
	836	969.185	Farm Access Road	Occupation	Signs	X S	Open	PRI
	836	969.575	Sugar Shed Road	Public Level	Flashing Lights		Open	LGA
	836	969.596		Tramway			Open	
	685	971.132		Tramway			Open	
	685	973.167	Farm Access Road	Occupation	Signs	X S	Open	PRI
	685	973.965	Walnal Road - Christoe Road	Public Level	Flashing Lights		Open	LGA
	685	975.256	Habana - Farleigh Road	Public Level	Flashing Lights		Open	LGA
	685	977.105	Knobels Road	Public Level	Signs	X S	Open	LGA
	685	978.632	Kochs Road	Public Level	Signs	X S	Open	LGA
	685	980.200	Farm Access Road	Occupation	Signs	X S	Open	PRI
	685	980.696	Mapalo Road	Public Level	Signs	X S	Open	LGA
	685	981.534	Farm Access Road	Occupation	Signs	X S	Open	PRI
	685	982.501	Old Leap Station Road	Public Level	Signs	X S	Open	LGA
	685	983.974	QR Maintenance Road	QR	Signs	X S	Open	QR
	685	989.659	Zunkers Road	Public Level	Flashing Lights		Open	LGA
	685	991.239		Occupation	Signs	X S	Open	PRI
	685	991.990		Occupation	Signs	X S	Open	LGA
	685	993.260	Edmunds Access Road	Occupation	Signs	X S	Open	PRI
	685	994.171	Off Hopfs Road	Occupation	Signs	X S	Open	PRI
	685	996.644		Tramway			Open	PRI
	685	996.654	Kuttabal / Mount Jukes Road (Seaforth Road)	Public Level	Half Boomgates		Open	LGA
	685	998.882	Geeberga Station Road	Public Level	Signs	X S	Open	LGA
	685	1002.053	Narpl School Road	Public Level	Flashing Lights		Open	LGA
	685	1004.050	Off Buthurra Road	Occupation	Signs	X S	Open	PRI
	685	1004.929		Occupation	Signs	X S	Open	PRI
	685	1008.531	Mount Ossa / Seaforth Road	Public Level	Half Boomgates		Open	LGA
	685	1011.374	Property Access Road	Occupation	Signs	X S	Open	PRI
	685	1012.478	Mount Pellon Road	Public Level	Flashing Lights		Open	LGA
	685	1017.660	Mc Dermotts Road	Public Level	Flashing Lights	PWB	Open	LGA
	685	1018.270	Old Bowen Road (St Helens Beach Road)	Public Level	Flashing Lights		Open	LGA
	685	1019.100	Mackenzies Road	Public Level	Flashing Lights		Open	LGA
	685	1020.820	Pindi Pindi Tramway	Tramway	Nil		Open	
	685	1020.866	Property Access Road	Occupation	Signs	X S	Open	PRI
	685	1023.353		Occupation	Signs	X S	Open	PRI

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685	1023.855	Blackrock Creek Road	Public Level	Flashing Lights		Open	LGA
685	1027.768	Wintons Road	Public Level	Flashing Lights	PWB	Open	LGA
685	1028.693	Wagoora - Yalboroo Access Rd.	Occupation	Signs	X S	Open	PRI
685	1030.029	Dumsford Road	Public Level	Signs	X S	Open	LGA
685	1031.150	Wagoora - Yalboroo Access Rd	Public Level	Signs	X S	Open	LGA
685	1033.651	Paskins Road	Public Level	Half Boomgates		Open	LGA
685	1037.090	Cathu O'donnell River Road	Public Level	Signs	X S	Open	LGA
685	1038.712		Public Level	Signs	X S	Open	LGA
685	1039.665	Wales Road	Public Level	Signs	X S	Open	LGA
685	1040.084	Wales Road Pedestrian Access	Pedestrian			Open	LGA
685	1044.449	Kamo Road	Public Level	Signs	X S	Open	LGA
685	1047.790	Forbes Road	Public Level	Signs	X S	Open	LGA
685	1049.775	Dohartys Road (Dump Access Road)	Public Level	Flashing Lights		Open	LGA
685	1052.704	O'donnells Road	Public Level	Signs	X S	Open	LGA
685	1059.367	Caping Road	Public Level	Signs	X S	Open	LGA
685	1065.270	Holcombe Road	Public Level	Signs	X S	Open	LGA
685	1068.479	Thoopara Road	Public Level	Flashing Lights	X	Open	LGA
685	1072.910	Gunyarra Road	Public Level	Flashing Lights	X	Open	LGA
685	1075.730	Airport Drive	Public Level	Flashing Lights	X	Open	LGA
685	1081.890	Property Access Road	Occupation	Signs	X S	Open	PRI
685	1083.800	Property Access Road	Occupation	Signs	X S	Open	PRI
685	1084.620	Anzac Road	Public Level	Half Boomgates	X	Open	LGA
843	1085.153	Main / Faust Street	Public Level	Half Boomgates	X	Open	MRD
843	1086.380		Occupation	Signs	X S	Open	PRI
843	1086.970		Occupation	Signs	X S	Open	PRI
843	1087.540	Property Access Road	Occupation	Signs	X S	Open	PRI
843	1088.181		Occupation	Signs	X S	Open	PRI
843	1089.100	Foxdale Diamond	Tramway			Open	PRI
843	1089.402		Occupation	Signs	X S	Open	PRI
843	1089.700		Occupation	Signs	X S	Open	PRI
843	1090.411	Up River Road	Public Level	Half Boomgates	X	Open	LGA
843	1091.562		Occupation	Signs	X S	Open	PRI
843	1096.100	Blair Road	Public Level	Signs	X S	Open	LGA
843	1097.360	Koolachu Road	Public Level	Signs	X S	Open	LGA
843	1111.420	Debella Yard Access Road	Occupation	Signs	X S	Open	PRI
843	1113.320		Occupation	Signs	X S	Open	PRI
843	1116.600	Gordon Glen Road	Public Level	Flashing Lights		Open	LGA
843	1118.260	Tom Jones Access Road	Public Level	Signs	X S	Open	LGA
843	1119.340		Occupation	Signs	X S	Open	PRI
843	1125.365		Occupation	Signs	X S	Open	PRI
843	1130.460	Intaburra Road	Public Level	Signs	X S	Open	LGA
843	1131.340	Roma Peak Road	Public Level	Signs	X S	Open	LGA
843	1132.230		Occupation	Signs	X S	Open	PRI
843	1133.690	Glen Erin Trail Rides Access Road	Occupation	Signs	X S	Open	PRI
843	1141.040	Maloney Lane	Public Level	Signs	X S	Open	LGA
843	1141.780	Bootooloo Road	Public Level	Flashing Lights	X	Open	LGA
843	1142.200	Off Bootooloo Road	Occupation	Signs	X S	Open	PRI
843	1142.630	Off Bootooloo Road	Occupation	Signs	X S	Open	PRI
843	1143.200	Off Bootooloo Road	Occupation	Signs	X S	Open	PRI

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843	1143.900	Leyton Road	Public Level	Signs	X S	Open	LGA
843	1144.540	Kellys Lane	Public Level	Signs	X S	Open	LGA
843	1144.765	Off Bootooloo Road	Occupation	Signs	X S	Open	PRI
843	1144.875	Off Bootooloo Road	Occupation	Signs	X S	Open	PRI
843	1145.450	Whealers Lane	Public Level	Signs	X S	Open	LGA
843	1145.750	Nursery Access Road	Occupation	Signs	X S	Open	PRI
843	1146.183	Bruce Highway (Big Red)	Public Level	Half Boomgates		Open	MRD
844	1154.140	Lauriston Street	Public Level	Flashing Lights		Open	MRD
849	1156.846	Linley Street	Public Level	Half Boomgates	X	Open	LGA
312	1164.130	Microwave Road	Occupation	Signs	X S	Open	PRI
312	1166.080	Abbott Point Road	Public Level	Flashing Lights	X	Open	LGA
312	1167.310		Occupation	Signs	X S	Open	PRI
312	1170.850		Occupation	Signs	X S	Open	PRI
312	1172.630	Mount Luce Station Road	Occupation	Signs	X S	Open	PRI
312	1175.900	Property Access Road	Occupation	Signs	X S	Open	PRI
312	1183.120	Bruce Highway	Public Level	Flashing Lights		Open	MRD
312	1185.714		Occupation	Signs	X S	Open	PRI
312	1190.870	Glenore Road	Public Level	Signs	X S	Open	LGA
312	1193.078		Occupation	Signs	X S	Open	PRI
312	1193.990	Nevada Road	Public Level	Flashing Lights	X	Open	LGA
312	1200.630		Occupation	Signs	X S	Open	PRI
312	1202.870		Occupation	Signs	X S	Open	PRI
312	1203.860	Opposite Cape Creek Road	Public Level	Signs	X S	Open	LGA
312	1209.340	Tondara Road	Public Level	Signs	X S	Open	LGA
312	1210.960	Desalis Street	Public Level	Flashing Lights		Open	LGA
312	1215.220	Gumlu Road	Public Level	Signs	X S	Open	LGA
312	1217.620	N.Q. Fruit And Veg. Access Road	Public Level	Signs	X S	Open	LGA
312	1218.578	Wakala Station Access Road	Occupation	Signs	X S	Open	PRI
312	1220.240	Rocky Ponds Road	Public Level	Signs	X S	Open	LGA
312	1225.010	Rangemore Road	Public Level	Flashing Lights		Open	LGA
312	1228.580		Occupation	Signs	X S	Open	PRI
312	1230.480	Property Access Road	Occupation	Signs	X S	Open	PRI
312	1232.070	Glenyarra Station Access Road	Occupation	Signs	X S	Open	PRI
312	1233.930	Access Road	Public Level	Signs	X S	Open	LGA
312	1236.800	Daniel Road	Public Level	Flashing Lights	X	Open	LGA
312	1238.800	Old Bowen Road / Bruce Highway	Public Level	Signs	X S	Open	LGA
312	1240.275	Fredericksfield Road	Public Level	Flashing Lights	X	Open	LGA
312	1243.070	Homestead Road	Public Level	Signs	X S	Open	LGA
312	1244.275	Georgees Road / Bruce Highway	Public Level	Signs	X S	Open	LGA
312	1245.410	Georgees Road / Bruce Highway	Public Level	Signs	X S	Open	LGA
312	1247.680	Georgees Road / Bruce Highway	Public Level	Signs	X S	Open	LGA
312	1248.641	First Street and Hurney Road	Public Level	Flashing Lights		Open	LGA
312	1249.229	Sixth Street	Public Level	Flashing Lights		Open	LGA
312	1249.760	Tenth Street	Public Level	Half Boomgates		Open	LGA
689	1250.335	Fourteenth A Street	Public Level	Flashing Lights		Open	LGA
689	1251.352	Bruce Highway (on siding)	Public Level	Flashing Lights		Open	MRD
689	1251.607	Milburn Road (on siding)	Public Level			Open	LGA
689	1251.610	Kirknie Road	Public Level	Flashing Lights		Open	LGA
689	1254.130	Clare Road	Public Level	Half Boomgates		Open	LGA

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689	1255.567	Giddy Road	Public Level	Flashing Lights		Open	LGA
689	1255.577		Tramway			Open	
689	1258.430	Mirrigan Road	Public Level	Flashing Lights		Open	LGA
689	1260.175	Drysdale Street	Public Level	Flashing Lights		Open	LGA
689	1260.996		QR	Nil		Open	
689	1261.340	Albert Street and Lynch Street	Public Level	Flashing Lights		Open	LGA
689	1262.360	Robins Road	Public Level	Signs	X S	Open	LGA
689	1263.180		Tramway			Open	
689	1265.370	Sexton Street Pedestrian Crossing	Pedestrian		X T	Open	LGA
689	1265.850	Spiller Street	Public Level	Flashing Lights	X	Open	LGA
689	1266.020	Bruce Highway	Public Level	Half Boomgates	X	Open	MRD
689	1266.510		Tramway			Open	PRI
689	1266.570	Drysdale Street	Public Level	Signs	X S	Open	LGA
689	1268.730	Drynie Road	Public Level	Flashing Lights	X	Open	LGA
689	1274.980	Lochinvar Road	Public Level	Signs	X S	Open	LGA
689	1279.510	Property Access Road	Occupation	Signs	X S	Open	PRI
689	1282.079	Jerona Road	Public Level	Signs	X S	Open	LGA
689	1290.720	Hodel Road	Public Level	Signs	X S	Open	LGA
689	1291.680	Mc Lennan Road	Public Level	Signs	X S	Open	LGA
689	1293.420	Walton Street	Public Level	Half Boomgates		Open	LGA
689	1294.816	Cromarty Road	Public Level	Signs	X S	Open	LGA
689	1300.340	Property Access Road	Occupation	Signs	X S	Open	PRI
689	1301.590		Occupation	Signs	X S	Open	PRI
689	1310.255	A.L.M.S. Road	Public Level	Flashing Lights		Open	LGA
689	1313.725		Occupation	Signs	X S	Open	PRI
689	1316.900	Marron Road	Public Level	Signs	X S	Open	LGA
689	1317.640	Allendale Station Access Road	Occupation	Signs	X S	Open	PRI
689	1320.380	Bentley Drive	Public Level	Flashing Lights		Open	LGA
689	1321.220	Off Nome Road	Public Level	Signs	X S	Open	LGA
480	1322.970	Muntalunga Access Road	Occupation	Signs	X S	Open	PRI
480	1325.484	Coast Road (Sunnyside Street)	Public Level	Signs	X S	Open	LGA
480	1327.314	Sunnyside Street	Public Level	Signs	X T	Open	LGA
480	1327.349	Julago Cattle Yard Access Road	Public Level	Signs	X T	Open	LGA
480	1327.410	Coast Road	Public Level	Signs	X S	Open	LGA
481	1329.483	Southwood Road	Public Level	Flashing Lights		Open	LGA
481	1329.518	Hunter Street	Public Level	Flashing Lights		Open	LGA
481	1330.058	Partington Yard	QR	Signs	X T	Open	QR
481	1330.582	Southwood Road	Pedestrian	Nil		Open	LGA
481	1330.610	Southwood Road	Public Level	Half Boomgates	PWB	Open	LGA
891	1331.529	Jensen Street	Public Level	Signs	X S	Open	LGA
891	1334.551	Racecourse Road (Bruce Highway)	Public Level	Half Boomgates		Open	MRD
891	1336.495	Oonoonba Road	Public Level	Half Boomgates	PWB	Open	LGA
891	1337.568	D.P.I. Access Road	Public Level	Half Boomgates		Open	LGA
891	1338.685	Railway Avenue	Pedestrian			Open	LGA
891	1339.187	Queens Road	Public Level	Half Boomgates		Open	MRD
891	1339.941	Boundary Street	Public Level	Flashing Lights		Open	MRD
852	1340.900	Charters Towers Road (Causeway)	Public Level	Half Boomgates		Open	LGA
852	1341.048	Sturt Street	Pedestrian			Open	LGA
852	1341.118	Sturt Street	Public Level	Half Boomgates		Open	LGA

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852	1341.523	Church Street	Public Level	Half Boomgates		Open	LGA
852	1343.145	Parkes Street	Pedestrian	Nil		Open	LGA
852	1343.690	Kings Road	Public Level	Half Boomgates	PWB	Open	LGA
852	1344.760	Hugh Street	Public Level	Half Boomgates		Open	LGA
852	1345.630	Bolam Street (on siding)	Public Level			Open	LGA
852	1345.760	Private Access Road (on siding)	Occupation			Open	PRI
852	1345.880	Private Access Road (on siding)	Occupation			Open	PRI
852	1345.912	Langton Street	Public Level	Half Boomgates	PWB	Open	LGA
852	1346.380	Pilkington Street	Public Level	Half Boomgates	X	Open	LGA
852	1347.130	Duckworth Street	Public Level	Half Boomgates		Open	LGA
852	1348.360	Mather Street	Public Level	Flashing Lights		Open	LGA
852	1350.964	Weston Street	Public Level	Half Boomgates		Open	LGA
852	1351.710	Commercial Street (on siding)	Public Level			Open	LGA
852	1351.760	Private Access Road (on siding)	Occupation			Open	PRI
852	1352.210	Enterprise Street (on siding)	Public Level			Open	LGA
852	1352.390	Utility Lane (on siding)	Public Level			Open	LGA
852	1352.400	Ingham Road	Public Level	Half Boomgates		Open	LGA
852	1353.400	Burdell Road (Campbell Coaches)	Public Level	Signs	X S G	Open	LGA
852	1355.200	Property Access road	Occupation	Signs	X S	Open	PRI
852	1355.960	Garland Road	Public Level	Half Boomgates		Open	MRD
852	1357.962	Mount Low Parkway	Public Level	Half Boomgates	X	Open	LGA
852	1359.164	Svensson Street	Public Level	Signs	X S G	Open	LGA
852	1361.345	Brabon Road	Public Level	Signs	X S	Open	LGA
852	1361.920	Bowden Road	Public Level	Flashing Lights		Open	LGA
852	1364.912	Greenvale Street	Public Level	Flashing Lights	X	Open	LGA
853	1366.082	Saunders Beach Road	Public Level	Flashing Lights		Open	LGA
853	1367.795	Purono Parkway Road	Public Level	Flashing Lights	X	Open	LGA
854	1370.959	Bluewater Drive	Public Level	Signs	X S	Open	LGA
854	1372.150	Toolakea Beach Road	Public Level	Signs	X S	Open	LGA
854	1376.633	Property Access Road	Occupation	Signs	X S	Open	PRI
854	1377.772	Property Access Road	Occupation	Signs	X S	Open	PRI
854	1378.285		Occupation	Signs	X S	Open	PRI
854	1379.781	Setter Road	Public Level	Signs	X T	Open	LGA
854	1380.450	Endevour Foundation Access Road	Occupation	Signs	X S	Open	PRI
854	1384.285		Occupation	Signs	X T	Open	PRI
854	1384.746	Toomulla Beach Road	Public Level	Signs	X S	Open	LGA
854	1389.471	Clemant State Forest Access Road	Public Level	Signs	X T	Open	LGA
854	1393.200	Balgil Beach Road	Public Level	Signs	X S	Open	LGA
854	1394.212	Rollingstone Station Pedestrian Access	Pedestrian			Open	LGA
854	1394.757	Rollingstone Street	Public Level	Signs	X S	Open	LGA
854	1398.368	Hencamp Creek Road	Public Level	Signs	X S	Open	LGA
854	1403.867	Ollera Creek Road	Public Level	Signs	X G	Open	LGA
854	1409.072	Round Road / Mutarne Road	Occupation	Signs	X T	Open	PRI
854	1409.839	Off Barnligle Road	Occupation	Signs	X T	Open	PRI
854	1410.670	Amos Road	Public Level	Signs	X S	Open	LGA
854	1412.850	Property Access Road	Occupation	Signs	X T	Open	PRI
854	1414.650	Suthers Road	Public Level	Signs	X T	Open	LGA
854	1415.844	Deehin Road	Public Level	Signs	X S	Open	LGA
854	1418.240		Occupation	Signs	X T	Open	PRI

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854	1421.520	Property Access Road	Occupation	Signs	X S	Open	PRI
854	1421.930	Abswold Road	Public Level	Signs	X T	Open	LGA
854	1424.370	Bambaroo Road	Public Level	Signs	X G	Open	LGA
854	1425.400	Property Access Road	Occupation	Signs	X T	Open	PRI
854	1426.188	Scrubview Road	Public Level	Flashing Lights	PWB	Open	LGA
854	1427.440	Yuruga Road	Public Level	Signs	X S	Open	LGA
854	1427.961		Occupation	Signs	X T	Open	PRI
854	1433.900		Occupation	Signs	X S	Open	PRI
854	1435.605	Helens Hill Road	Public Level	Signs	X T	Open	LGA
854	1436.669	Railway Road / Pombel Road	Public Level	Signs	X T	Open	LGA
854	1437.083		Tramway			Open	PRI
854	1438.160	Railway Road / Haughty Road	Public Level	Signs	X T	Open	LGA
854	1440.526	Pinnacle Hill Road	Public Level	Signs	X S	Open	LGA
854	1441.279	Property Access Road	Occupation	Signs	X T	Open	PRI
854	1441.690	Masters Road	Public Level	Signs	X S	Open	LGA
854	1442.671	Scovazzis Road	Public Level	Signs	X S	Open	LGA
854	1443.725		Occupation	Signs	X T	Open	PRI
854	1444.826	Tokalon Road	Public Level	Signs	X S	Open	LGA
854	1447.470	Dickson Street	Public Level	Signs	X S	Open	LGA
854	1447.868	Morehead Street / Showgrounds Access	Public Level	Signs	X T	Open	LGA
854	1447.920	Showgrounds Pedestrian Access	Pedestrian	Signs	X T	Open	PRI
854	1448.030	Showgrounds Pedestrian Access	Pedestrian	Signs	X T	Open	PRI
854	1448.090	Showgrounds Pedestrian Access	Pedestrian	Signs	X T	Open	PRI
854	1448.200	Cooper Street	Public Level	Signs	X S	Open	LGA
854	1448.580	Mollwraith Street	Public Level	Flashing Lights		Open	LGA
854	1449.110	Selby Lane / Anne Street	Public Level	Signs	X T	Open	LGA
854	1449.250	Cartwright Street	Public Level	Flashing Lights		Open	MRD
854	1449.270		Tramway	Nil		Open	PRI
854	1449.367	QR Pedestrian Traffic Only	Pedestrian	Nil		Open	QR
854	1449.560	QR Station Yard Access Road	QR	Nil		Open	QR
855	1452.058	Sachs Lane	Public Level	Signs	X S	Open	LGA
855	1452.080		Tramway	Nil		Open	PRI
855	1453.454	Private Access Road	Occupation	Signs	X G	Open	PRI
855	1453.685	Forest Home Road	Public Level	Signs	X S	Open	LGA
855	1454.469	Private Access Road	Occupation	Signs	X S	Open	PRI
855	1455.200	Private Access Road	Occupation	Signs	X G	Open	PRI
855	1455.515	Private Access Road	Occupation	Signs	X G	Open	PRI
855	1456.400	Fraochlas Road	Public Level	Signs	X T	Open	LGA
855	1457.393	Ingham - Halifax Road	Public Level	Flashing Lights	X	Open	MRD
855	1457.606		Tramway	Nil		Open	PRI
855	1457.729	Farm Access Road	Occupation	Signs	X T	Open	PRI
855	1458.066	Farm Access Road	Occupation	Signs	X T	Open	PRI
855	1459.137	Macmillans Street	Occupation	Signs	X S	Open	PRI
855	1459.163		Tramway			Open	PRI
855	1459.759	Halifax - Lucinda Point Road	Public Level	Flashing Lights		Open	MRD
855	1460.212	Girgentis Road	Public Level	Signs	X G	Open	LGA
855	1460.225		Tramway	Nil		Open	PRI
855	1462.192	Access Road	Occupation	Signs	X S	Open	PRI

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855	1462.712	Access Road	Occupation	Signs	X T	Open	PRI
855	1464.150	Unnamed Road	Public Level	Signs	X T	Open	LGA
855	1466.532	QR Maintenance Road	QR	Nil		Open	QR
855	1468.963	Bruce Highway	Public Level	Flashing Lights		Open	MRD
855	1470.537	Sunday Creek Road	Occupation	Signs	X G	Open	PRI
855	1476.451	State Forest Access Road	Occupation	Signs	X T	Open	PRI
855	1477.188	Bruce Highway	Public Level	Flashing Lights		Open	MRD
855	1488.629	Forestry Access Road	Occupation	Signs	G	Open	PRI
855	1489.642	Melclouds Road	Occupation	Signs	X G	Open	PRI
855	1493.459	Nicholson Road	Occupation	Signs	X S	Open	PRI
855	1495.886	Seafarm Aquaculture Access Road	Occupation	Signs	X G	Open	PRI
855	1497.524	Tullamore Road	Public Level	Signs	X G	Open	LGA
855	1498.465	Private Access Road	Occupation	Signs	X G	Open	PRI
855	1499.920	Port Hinchinbrook Access Road	Occupation	Signs	X G	Open	PRI
855	1500.848	Bruce Highway	Public Level	Flashing Lights		Open	MRD
855	1502.478	QR Pedestrian Access Road	Pedestrian			Open	QR
855	1502.644	Brasenose Street	Public Level	Signs	X S	Open	LGA
855	1502.883	Bowen Street Pedestrian Access	Pedestrian			Open	LGA
855	1503.124	Liverpool Street	Public Level	Signs	X S	Open	LGA
855	1504.844	Cemetery Road	Public Level	Signs	X S	Open	LGA
855	1507.516	Ellerbeck Road	Public Level	Flashing Lights		Open	LGA
855	1508.433	Farm Access Road	Occupation	Signs	X G	Open	PRI
855	1509.255	Lily Creek Road	Public Level	Signs	X S	Open	LGA
855	1510.493	Property Access Road	Occupation	Signs	X T	Open	PRI
855	1513.011	Kennedy Creek Road	Public Level	Flashing Lights		Open	LGA
855	1518.317	Lycsee Farm Access Road	Public Level	Signs	X S	Open	LGA
855	1519.048	Old Cardwell Road	Public Level	Signs	X T	Open	LGA
855	1524.297	Bilyana Road	Public Level	Flashing Lights		Open	LGA
855	1526.620	Private Access Road	Occupation	Signs	X S	Open	PRI
855	1529.726	Murrigal Road	Occupation	Signs	X S	Open	PRI
855	1530.468	Murray Upper Road	Public Level	Flashing Lights		Open	MRD
855	1531.919	Bellenden Road	Public Level	Signs	X S	Open	LGA
855	1535.119	Farm Access Road	Occupation	Signs	X G	Open	PRI
855	1535.524	Farm Access Road	Occupation	Signs	X G	Open	PRI
855	1536.043	Menzels Road	Public Level	Signs	X G	Open	LGA
855	1537.240	Davidsons Road	Tramway			Open	PRI
855	1537.254	Davidson Road	Public Level	Signs	X S	Open	MRD
855	1537.921	Farm Access Road	Occupation	Signs	X G	Open	PRI
855	1538.499	Cargnello Road	Public Level	Signs	X T	Open	LGA
855	1539.879	Farm Access Road	Occupation	Signs	X G	Open	PRI
855	1541.146	Farm Access Road	Occupation	Signs	X G	Open	PRI
855	1541.680		Tramway	Nil		Open	PRI
855	1542.105	Dallachy Road	Public Level	Flashing Lights		Open	LGA
855	1543.777	Muldoon Road	Public Level	Flashing Lights		Open	LGA
855	1544.622	High School Access Road	Public Level	Signs	X S	Open	LGA
855	1544.719	High School Access Road	Public Level	Signs	X S	Open	LGA
855	1545.313	Jarra Creek Road / Butler Street	Public Level	Flashing Lights		Open	LGA
855	1546.424	Vaughan Street	Public Level	Signs	X S	Open	LGA
855	1546.863	Old Tully Road	Public Level	Flashing Lights		Open	LGA

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855	1547.475	Private Access Road	Occupation	Signs	X S	Open	PRI
855	1548.105		Tramway	Nil		Open	
855	1549.144	Private Access Road	Occupation	Signs	X G	Open	PRI
855	1550.567	Midgenoo Road	Public Level	Signs	X S	Open	LGA
855	1551.692	Private Access Road	Occupation	Signs	X G	Open	PRI
855	1552.092	Private Access Road	Occupation	Signs	X S	Open	PRI
855	1552.733	Feluga Road	Public Level	Signs	X S	Open	LGA
855	1552.760		Tramway	Nil		Open	
855	1553.192	Private Access Road	Occupation	Signs	X G	Open	PRI
855	1553.802	Private Access Road	Occupation	Signs	X G	Open	PRI
855	1554.058	Old Tully Road	Public Level	Signs	X S	Open	LGA
855	1554.613	Access Road	Occupation	Signs	X G	Open	PRI
855	1557.743	QR Maintenance Road	QR	Signs		Open	QR
855	1559.544	Old Tully Road	Public Level	Signs	X S	Open	LGA
855	1560.055	Private Access Road	Occupation	Signs	X G	Open	PRI
855	1560.085		Tramway	Nil		Open	
855	1560.314	Old Tully Road	Public Level	Signs	X S	Open	LGA
855	1560.592	Private Access Road	Occupation	Signs	X G	Open	PRI
855	1561.789	Royston Street Pedestrian Crossing	Pedestrian	Nil		Open	LGA
855	1562.172	Glasgow Street	Public Level	Signs	X S	Open	LGA
855	1564.274	Private Access Road	Occupation	Signs	X G	Open	PRI
855	1564.565		Tramway	Nil		Open	
855	1564.571	Kings Road	Public Level	Signs	X S	Open	LGA
855	1567.194	Farm Access Road	Occupation	Signs	X G	Open	PRI
855	1568.793	Silkwood - Japoon Road	Public Level	Flashing Lights		Open	MRD
855	1568.805		Tramway	Nil		Open	
855	1568.820	Victoria Street	Public Level	Signs	X S	Open	LGA
855	1569.759	Farm Access Road	Occupation	Signs	X G	Open	PRI
855	1570.521	Petersons Road	Public Level	Signs	X T	Open	LGA
855	1571.298	Farm Access Road	Occupation	Signs	X G	Open	PRI
855	1572.245	Farm Access Road	Occupation	Signs	X G	Open	PRI
855	1573.409	Warrubullen Road	Public Level	Signs	X S	Open	LGA
855	1573.870	Farm Access Road	Occupation	Signs	X G	Open	PRI
855	1574.210	Property Access Road	Occupation			Open	PRI
855	1575.056	Warrubullen Road	Public Level	Signs	X G	Open	LGA
855	1575.292	Private Access Road	Occupation	Signs	X S	Open	PRI
855	1575.933	Dobbin Road	Public Level	Signs	X G	Open	LGA
855	1578.952	Private Access Road	Occupation	Signs	X S	Open	PRI
855	1579.603	Private Access Road	Occupation	Signs	X G	Open	PRI
855	1580.613	Valmadre Road	Public Level	Signs	X T	Open	LGA
855	1581.623	Farm Access Road	Occupation	Signs	X G	Open	PRI
855	1581.866	Private Access Road	Occupation	Signs	X G	Open	PRI
855	1581.870		Tramway	Nil		Open	
855	1582.784	Private Access Road	Occupation	Signs	X G	Open	PRI
855	1583.267	Cataldo Road	Public Level	Signs	X S	Open	LGA
855	1583.280	Cataldo Road	Tramway	Nil		Open	
855	1584.107	Sorbello Road	Public Level	Signs	X T	Open	LGA
857	1585.010	South Johnstone Road	Tramway	Nil		Open	
857	1585.014	South Johnstone Road	Public Level	Flashing Lights	X	Open	MRD

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857	1585.977	McIlraith Road	Public Level	Signs	X G	Open	LGA
857	1586.607	Access Road	Occupation	Signs	X G	Open	PRI
857	1587.602	Access Road	Occupation	Signs	X G	Open	PRI
857	1588.177	Property Access Road	Occupation	Signs	X S	Open	PRI
857	1588.641	Mayer Avenue / Innisfail Japoon Road	Public Level	Flashing Lights		Open	MRD
857	1588.941		Tramway	Nil		Open	
857	1589.785		Tramway	Nil		Open	
857	1590.447	Aerodrome Road	Public Level	Signs	X S	Open	LGA
857	1591.251	Douglas Road	Public Level	Signs	X G	Open	LGA
857	1591.567	Access Road	Occupation	Signs	X G	Open	PRI
857	1592.200	Access Road (from Backhaus Street)	Occupation	Signs	X G	Open	PRI
857	1592.808	Access Road	Occupation	Signs	X G	Open	PRI
857	1593.456	QR Maintenance Road	QR	Signs	X S	Open	QR
857	1593.562	QR Yard Crossing	QR	Signs	X S	Open	QR
417	1594.321	Bruce Highway / Palmerston Drive	Public Level	Flashing Lights		Open	MRD
417	1594.622	Power Street (Danielle Street)	Public Level	Flashing Lights		Open	LGA
417	1595.922	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1596.380	See Poy Road	Public Level	Flashing Lights	PWB	Open	LGA
417	1596.384		Tramway	Signs	X S T	Open	PRI
417	1597.633	Old Ferry Road	Occupation	Signs	X S	Open	PRI
417	1598.839	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1599.786	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1600.109		Tramway	Nil		Open	
417	1600.157	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1601.018	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1601.670		Tramway	Nil		Open	
417	1601.687	Garradunga Road	Public Level	Signs	X S T	Open	LGA
417	1602.227	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1602.525	Property Access Road	Occupation	Signs	X T	Open	PRI
417	1602.913	Mamu Road	Public Level	Signs	X S	Open	LGA
417	1604.204	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1604.547	Goldmine Road	Public Level	Signs	X T	Open	LGA
417	1605.225	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1608.341	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1611.080	QR Maintenance Road	QR	Nil		Open	QR
417	1611.331	Roper Road	Public Level	Signs	X S	Open	LGA
417	1613.122	opp Hoskin Road	Public Level	Signs	X S	Open	LGA
417	1613.579	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1614.099	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1614.829	Bramston Beach Road	Public Level	Flashing Lights		Open	LGA
417	1614.850	Bramston Beach Road	Tramway	Nil		Open	
417	1615.726	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1616.695	Musumeel Road	Public Level	Signs	X T	Open	LGA
417	1617.695	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1619.249	Lloyd Road	Public Level	Signs	X G T	Open	LGA
417	1620.013	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1620.711	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1620.911	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1621.375	Old Bruce Highway	Public Level	Signs	X T	Open	LGA

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417	1621.694	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1622.146	Munro Street	Public Level	Flashing Lights	PWB	Open	MRD
417	1623.097	Christian Street	Public Level	Signs	X G	Open	LGA
417	1623.450		Tramway	Nil		Open	
417	1624.232	Nelson Road	Public Level	Signs	X S	Open	LGA
417	1625.135	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1625.321	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1625.865	Property Access Road	Occupation	Signs		Open	PRI
417	1626.417	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1626.767	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1626.935	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1627.322	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1627.506	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1628.869	Unnamed Road	Public Level	Signs	X S	Open	LGA
417	1629.174	Mustafa Road	Public Level	Signs	X G	Open	LGA
417	1629.720		Tramway	Nil		Open	
417	1629.832	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1630.488	Russell River Road	Public Level	Signs	X S	Open	LGA
417	1632.015	Ellis Road	Public Level	Signs	X G	Open	LGA
417	1632.864	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1633.796	Harvey Creek Road	Public Level	Signs	X G	Open	LGA
417	1634.806	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1635.086	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1635.835	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1636.894	Ross Road	Public Level	Signs	X G T	Open	LGA
417	1637.280	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1638.266	Lewis Road	Public Level	Signs	X S	Open	LGA
417	1638.666	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1638.918	Property Access Road	Occupation	Signs	X T	Open	PRI
417	1639.266	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1639.883	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1640.266	Unnamed Road	Public Level	Signs	X T	Open	LGA
417	1640.739	Unnamed Road	Public Level	Signs	X G T	Open	LGA
417	1641.522	Stewart Road	Public Level	Signs	X G	Open	LGA
417	1642.400	Tidmarsh Road	Public Level	Signs	X G T	Open	LGA
417	1643.189	Property Access Road	Occupation	Signs	X G T	Open	PRI
417	1643.846	Property Access Road	Occupation	Signs	X T	Open	PRI
417	1644.631	Cane Bin Haulage Road	Occupation	Signs	X S	Open	PRI
417	1644.760	Grimshaw Road	Public Level	Signs	X S	Open	LGA
417	1645.523	McNab Road	Public Level	Signs	X S	Open	LGA
417	1646.541	Meerawa Road	Public Level	Signs	X G T	Open	LGA
417	1647.950	Assmen / Alobomba Road	Public Level	Signs	X G	Open	LGA
417	1648.962	Property Access Road	Occupation	Signs	X G	Open	PRI
417	1649.484	Molier Road	Public Level	Signs	X T	Open	LGA
417	1649.850		Occupation	Signs	X S	Open	PRI
417	1650.827	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1651.569	Hesp / Bennett Road	Public Level	Signs	X S	Open	LGA
417	1652.552	Kenny Road	Public Level	Signs	X G	Open	LGA
417	1652.731	Property Access Road	Occupation	Signs	X S	Open	PRI

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<u>Line Section Code</u>	<u>Km</u>	<u>Road Name</u>	<u>Type</u>	<u>Vehicular Protection</u>	<u>Signs</u>	<u>Open Status</u>	<u>Responsible Authority</u>
417	1653.430		Tramway	Nil		Open	
417	1654.311	Cane Sliding Access road	Occupation	Signs	X S	Open	PRI
417	1654.680	Banna Road	Public Level	Signs	X T	Open	LGA
417	1655.209	Property Access road	Occupation	Signs	X S	Open	PRI
417	1656.057	Old Bruce Highway	Public Level	Signs	X G T	Open	LGA
417	1656.684	Old Bruce Highway	Public Level	Signs	X G	Open	LGA
417	1656.770		Tramway	Nil		Open	
417	1658.140		Tramway	Nil		Open	
417	1658.522	Gordon Street	Public Level	Flashing Lights		Open	LGA
417	1658.679	Sheppards Street Pedestrian Access	Pedestrian			Open	LGA
417	1658.961	Norman Street	Public Level	Flashing Lights		Open	LGA
417	1659.300	Pedestrian Access Road	Pedestrian			Open	LGA
417	1659.770	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1660.202	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1660.589	Maher Road	Public Level	Signs	X S G	Open	LGA
417	1661.128	Unnamed Road	Public Level	Signs	X S	Open	LGA
417	1661.867	Meringa Sugar Experiment Station Access Road	Occupation	Signs	X S	Open	PRI
417	1662.210	Hall Road	Public Level	Signs	X S	Open	LGA
417	1662.604	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1663.069	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1663.951	Warner Road	Public Level	Flashing Lights		Open	MRD
417	1664.411	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1665.318	Hill Road	Public Level	Signs	X S T	Open	LGA
417	1666.020	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1666.766	Deppeller Road	Public Level	Signs	X S	Open	LGA
417	1667.054	Property Access Road	Occupation	Signs	X S	Open	PRI
417	1667.313	Property Access Road	Occupation	Nil		Open	PRI
417	1668.590	Thomson Road	Public Level	Flashing Lights		Open	LGA
417	1669.485	Bentley Park Access Road	Occupation	Signs	X S	Open	PRI
417	1670.223	Swallow Road	Public Level	Flashing Lights		Open	LGA
417	1671.272	Progress Road	Public Level	Flashing Lights		Open	LGA
417	1672.106	Foster Road	Pedestrian	Nil		Open	MRD
417	1672.310	Coombs Street	Public Level	Flashing Lights		Open	LGA
417	1673.390	Progress Road	Public Level	Flashing Lights		Open	LGA
417	1674.435	Anderson Road	Public Level	Flashing Lights		Open	LGA
417	1674.569	Property Access Road	Occupation	Signs	X S	Open	PRI
315	1675.223	QR Pedestrian Access	Pedestrian	Nil		Open	QR
315	1676.264	Kate Street	Public Level	Flashing Lights		Open	LGA
315	1676.894	QR Maintenance Road	QR	Nil		Open	QR
315	1678.386	Aumuller Street	Public Level	Flashing Lights		Open	LGA
316	1679.489	Draper Street	Public Level	Flashing Lights		Open	LGA
316	1680.134	Dutton Street	Public Level	Flashing Lights		Open	LGA
316	1680.189	Kenny Street	Public Level	Half Boomgates		Open	LGA
316	1680.750	Spence Street	Public Level	Half Boomgates		Open	LGA
316	1681.190	Aplin Street	Public Level	Half Boomgates		Open	LGA

PHOSPHATE LOOP

487	0.585	Hubert Street				Open	
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<u>Line Section Code</u>	<u>Km</u>	<u>Road Name</u>	<u>Type</u>	<u>Vehicular Protection</u>	<u>Signs</u>	<u>Open Status</u>	<u>Responsible Authority</u>
TOWNSVILLE FORK							
136	-0.040	QR Access Road	QR	Nil		Open	QR
TOWNSVILLE HARBOUR BALLOON LOOP							
490	0.300	Benwell Road	Public Level	Signs	X S	Open	LGA
490	0.900		Occupation	Signs	S	Open	
490	1.855		Occupation			Open	
490	2.335		Occupation	Signs	X S	Open	
490	3.350	Lennon Drive (on siding)	Public Level			Open	LGA
TOWNSVILLE JETTY BRANCH							
489	0.570		QR			Open	
489	1.465	Morey Street	Public Level	Flashing Lights		Open	LGA
489	1.695	Allen Street	Public Level	Signs	X S	Open	LGA
489	1.925	Cannan Street	Pedestrian	Nil		Open	LGA
489	2.155	Archer Street	Public Level	Signs	X S	Open	LGA
489	2.185	Archer Street (on siding)	Public Level	Signs	X S	Open	LGA
TOWNSVILLE JETTY FORK NORTH							
144	0.232	QR Access Road	QR	Signs	X S	Open	QR
YEPPOON BRANCH							
410	0.050	Glenmore Road	Public Level	Half Boomgates		Open	LGA
410	0.178	Horace Street	Public Level	Signs	X S	Open	LGA
410	0.500	Fraser Street Pedestrian Access	Pedestrian			Open	LGA
410	0.817	Bridge Street	Public Level	Flashing Lights		Open	LGA
410	1.041	Fitzroy Bridge Pedestrian Access	Pedestrian			Open	LGA
410	1.423	Goodsall Street	Public Level	Signs	X S	Open	LGA
410	2.031		Occupation	Signs	X T	Open	
410	2.560	Dump Road	Public Level	Flashing Lights		Open	LGA
410	3.680		Occupation	Signs	X S	Open	PRI
410	4.613	Fitzroy River Access Road	Public Level	Signs	X S	Open	LGA
410	6.835	Lakes Creek Road	Public Level	Flashing Lights		Open	MRD
410	8.040	Hartington Street	Public Level	Signs	X S	Open	LGA
410	8.720	Barkers Road	Public Level	Signs	X S	Open	LGA
410	9.717	Nerimbera School Road	Public Level	Signs	X S	Open	LGA
410	10.911		Occupation	Nil		Open	PRI
411	11.377	Emu Park Road	Public Level	Flashing Lights		Open	MRD
411	12.834	Broadmeadows Access Road	Occupation	Signs	X S	Open	PRI
411	15.831	Thomson Point Road	Public Level	Signs	X S	Open	LGA
411	17.005		Occupation	Nil		Open	
411	18.650		Pedestrian			Open	PRI
411	18.730		Occupation	Signs	X S	Open	
411	18.860		Occupation	Signs	X S	Open	PRI
411	19.450		Occupation	Signs	X S	Open	
411	21.330		Occupation			Open	

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<u>Line Section Code</u>	<u>Km</u>	<u>Road Name</u>	<u>Type</u>	<u>Vehicular Protection</u>	<u>Signs</u>	<u>Open Status</u>	<u>Responsible Authority</u>
411	22.685		Occupation	Signs	X S	Open	PRI
411	23.000		Occupation	Signs	X S	Open	
411	23.990		Occupation	Signs	X S	Open	QR
411	25.260	Proposed Property Access Road	Occupation			Proposed	PRI
411	25.494	New Zealand Gully Road	Public Level	Signs	X S	Open	LGA
411	26.420		Occupation	NII		Open	
411	27.250	Wood Street	QR	Signs	X T	Open	QR
411	27.340	Sleipner Road	Public Level	Signs	X S	Open	LGA
411	28.203	Harold Road	Public Level	Signs	X S	Open	LGA
411	28.550	Botos Road	Public Level	Signs	X S	Open	LGA
411	32.066	Private Road	Occupation	Signs	X S	Open	PRI
411	33.104	Duncan Lane	Public Level	Signs	X S	Open	LGA
411	34.611	Dairy Inn Road / Annie Drive	Public Level	Signs	X S	Open	LGA
411	36.840	Tookers Road	Public Level	Signs	X T	Open	LGA
411	39.350		Occupation	Signs	X T	Open	PRI
411	40.875		Occupation			Open	
411	41.238	Property Access Road	Occupation	Signs	X S	Open	PRI
411	43.250		Occupation	NII		Open	PRI
411	44.850	Bondoola Road	Public Level	Signs	X S	Open	LGA
411	48.809	Rockhampton / Yeppoon Road	Public Level	Signs	X	Open	MRD
411	49.520	Golf Club	Pedestrian	NII	PWB	Open	PRI
411	50.836	Barmaryee Road	Public Level	Signs	X T	Open	LGA
411	53.170		Pedestrian	NII	PWB	Open	LGA
411	53.180	Braithwaite Street	Public Level	Signs	X T	Open	LGA
411	53.190	Jefferies Street (on siding)	Occupation	Signs	X T	Open	PRI
411	53.275	Pineapple Growers Entrance	Occupation	NII		Open	PRI
411	53.415	Ben Street (on siding)	Public Level	Signs	X T	Open	LGA
411	53.460		QR	NII		Open	QR
411	53.536	Yeppoon Station Pedestrian Crossing	Pedestrian	NII		Open	LGA

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APPENDIX D

Speed Boards

LOCATION OF SPEED BOARDS

TRACK	DISTANCE		FEATURE	EXISTING SPEED BOARDS	
	km	metres		UP TRAIN TO	DOWN TRAIN TO
	638	790	ROCKHAMPTON		
SINGLE	639	312			25
SINGLE	642	192		25	R25/50
SINGLE	643	95		L25/50	50
SINGLE	644	247		50	80
MAIN	648	325		80	R25/80
MAIN	648	892	PARKHURST		
SINGLE	649	473		L25/80	80
SINGLE	650	109			100
SINGLE	651	3		80	
SINGLE	652	100		T100	T120
SINGLE	654	750			T100
SINGLE	655	230		T120	
SINGLE	657	645		100	90
SINGLE	658	355		90	60
SINGLE	658	795		60	90
SINGLE	660	39		90	100
SINGLE	662	831		100	70
SINGLE	663	297		70	60
SINGLE	663	998		60	R50/60
MAIN	664	620		60	100
LOOP	664	871		50	25
SINGLE	665	5		80/25L	100
SINGLE	666	563		80	
SINGLE	673	450			80
SINGLE	673	709		100	
SINGLE	674	450		80	
SINGLE	676	539		100	80/25R
MAIN	676	631	YAAMBA		
MAIN	677	254		80	70
SINGLE	677	545		L25/70	70
SINGLE	677	724		70	100, T120
SINGLE	678	792		80	
SINGLE	683	0		T120	T160
SINGLE	687	952		100, T160	90
SINGLE	688	352		90	100, T140
SINGLE	691	689		100, T140	60
SINGLE	692	92		60	80
SINGLE	694	130		90	70
SINGLE	694	469			R25/70
SINGLE	694	480		70	
MAIN	694	600			80
MAIN	694	640	GLENGEDDES		
SINGLE	695	422		L25/80	90
SINGLE	695	811			100
SINGLE	696	747		80	
SINGLE	701	698		100	60
SINGLE	701	967		60	90
SINGLE	702	880		90	100, T120
SINGLE	705	100		T120	
SINGLE	706	480			T120
SINGLE	709	612		T120	
SINGLE	709	712			T120
SINGLE	711	129			80, T120
SINGLE	712	433		100, T120	L25/80
MAIN	713	20	KUNWARARA		
SINGLE	713	319		R25/80	100
SINGLE	715	400		80	
SINGLE	715	799			90
SINGLE	716	166		90	100
SINGLE	717	563		100	60
SINGLE	718	194		60	70
SINGLE	719	121		70	60
SINGLE	720	417		60	50
SINGLE	721	151		50	90
SINGLE	722	369		90	60
SINGLE	723	109		60	50
SINGLE	723	347		50	60

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SINGLE	724	479	60	90
SINGLE	725	873	90	80
SINGLE	727	605	80	R25/80
MAIN	728	410	PRINCHESTER	
SINGLE	728	507	L25/80	90
SINGLE	729	366		100
SINGLE	730	485	80	
SINGLE	731	438	100	90
SINGLE	731	809	90	100
SINGLE	734	70	90	
SINGLE	734	466	80	
SINGLE	734	955		90, T110
SINGLE	735	229	90, T110	100, T130
SINGLE	736	815	100, T130	90, T110
SINGLE	737	537	90, T110	100, T130
SINGLE	740	540	T130	
SINGLE	740	590		80
SINGLE	741	898	100	L25/80
MAIN	742	660	MARLBOROUGH	
SINGLE	742	833	R25/80	100
SINGLE	744	71	80	
SINGLE	745	659	100	90
SINGLE	746	374	90	70
SINGLE	747	318	70	50
SINGLE	747	530	50	
SINGLE	747	574		80
SINGLE	747	902	80	90
SINGLE	750	557	100	
SINGLE	751	372	90	80
SINGLE	751	712	80	100
SINGLE	755	660	100	80
SINGLE	756	180	80	90
SINGLE	758	353		80
SINGLE	759	676	90	R25/80
MAIN	760	386	KOOLTANDRA	
SINGLE	760	626	L25/80	90
SINGLE	761	19		100
SINGLE	761	951	80	
SINGLE	774	200		80
SINGLE	775	460	100	
SINGLE	775	584		R25/80
MAIN	775	910	OGMORE	
SINGLE	776	464	L25/80	100
SINGLE	777	879	80	
SINGLE	779	15	100	80
SINGLE	781	363	80	100
SINGLE	784	281	100	90
SINGLE	785	212	90	100
SINGLE	790	254	100	60
SINGLE	792	229	60	80
SINGLE	793	395	80	60
SINGLE	793	625	60	R25/80
MAIN	794	140	WUMALGI	
SINGLE	794	528	L25/80	80
SINGLE	795	400	80	60
SINGLE	797	681	60	70
SINGLE	798	471	70	80
SINGLE	800	426	80	100
SINGLE	809	479		80
SINGLE	810	917	100	L25/80
SINGLE	811	985	R25/80	80

EXISTING SPEED BOARDS

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TRACK	DISTANCE		FEATURE	UP TRAIN	DOWN TRAIN
	km	metres		TO BRISBANE	TO CAIRNS
SINGLE	813	31		60	100, T140
SINGLE	822	657		T140	T120
SINGLE	824	311		T120	
SINGLE	824	544		100	90
SINGLE	825	6		90	100
SINGLE	831	570			80
SINGLE	832	872		100	R25/80
MAIN	833	570	KALARKA		
SINGLE	833	807		L25/80	80
SINGLE	834	820		80	60
SINGLE	836	556		60	80
SINGLE	838	100	CLAIRVIEW		
SINGLE	838	495			70
SINGLE	838	785		70	80
SINGLE	840	483		80	60
SINGLE	841	662		60	100
SINGLE	846	806		100	R25/100
MAIN	847	330	ELALIE		
SINGLE	847	683		L25/100	100, T160
SINGLE	857	0			T100
SINGLE	857	417		T160	
SINGLE	858	816		T100	T140
SINGLE	864	17		100, T140	R25/80
MAIN	864	740	CARMILA		
SINGLE	864	897		L25/80	60
SINGLE	865	252		60	80
SINGLE	865	712			100
SINGLE	867	461		80	
SINGLE	874	775			80
SINGLE	875	675		100	L25/80
MAIN	876	40	ORKABIE		
SINGLE	876	579		R25/80	60
SINGLE	877	668		60	100
SINGLE	879	162		70	
SINGLE	880	951		100	90
SINGLE	882	767		90	60
SINGLE	882	916		60	80
SINGLE	884	633		80	60
SINGLE	886	736		60	80
SINGLE	889	490		100	R25/80
MAIN	889	600	ILBILBIE		
SINGLE	890	352		L25/80	80
SINGLE	891	779		80	50
SINGLE	892	508		50	80
SINGLE	893	450		80	90
SINGLE	894	488		90	100
SINGLE	897	894			90
SINGLE	897	956		90	
SINGLE	899	717		90	100
SINGLE	903	621			80
SINGLE	905	29		100	L25/80
MAIN	905	520	KOUMALA		
MAIN	905	774		80	60
SINGLE	905	969		R25/60	50
SINGLE	906	100		50	100
SINGLE	908	36			80
SINGLE	908	353		80	100
SINGLE	914	327			80
SINGLE	915	92		100	L25/80
SINGLE	915	208			L25/80
MAIN	915	980	YUKAN		
SINGLE	916	101		R25/80	80
SINGLE	916	519		R25/80	100
SINGLE	917	547		80	
SINGLE	920	951			80
SINGLE	921	359		100	
SINGLE	921	863		80	
SINGLE	924	53		80	
SINGLE	924	230			80
SINGLE	926	213		100	L25/60
MAIN	926	570	SARINA		
SINGLE	927	162		R25/60	80
SINGLE	930	599			100
SINGLE	931	916		80	
SINGLE	934	55			80
SINGLE	935	482		100	L25/80
MAIN	936	10	DAWLISH		
SINGLE	936	368		R25/80	80

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TRACK	DISTANCE		FEATURE	EXISTING SPEED BOARDS	
	km	metres		UP TRAIN TO BRISBANE	DOWN TRAIN TO CAIRNS
SINGLE	937	128			100
SINGLE	937	746		80	
SINGLE	941	48			80
SINGLE	942	395		100	R25/80
MAIN	943	282		L25/80	
SINGLE	943	312			80
SINGLE	944	780		80	100
SINGLE	948	530			80
SINGLE	950	241		100	L25/80
MAIN	950	796			70
SINGLE	951	158		R25/80	100
SINGLE	951	150	ROSELLA		
SINGLE	952	300		80	
SINGLE	953	170			90
SINGLE	954	199		90	
SINGLE	954	791		80	90
SINGLE	955	714			80
SINGLE	956	94		90	
SINGLE	957	320		100	
SINGLE	957	779		70	R25/70
MAIN	958	106		70	L25
MAIN	958	389		70	25
MAIN	959	0	MACKAY		
SINGLE	959	1011		25	100
SINGLE	961	845		80	
SINGLE	963	424			80
SINGLE	969	618		100	
SINGLE	970	3		80	R50/80
SINGLE	970	500	ERAKALA		
SINGLE	970	500	ERAKALA		
SINGLE	971	50		L25/80	80
SINGLE	971	141			90
SINGLE	971	444			100
SINGLE	972	346		80	
SINGLE	972	678			80
SINGLE	974	236		80	L25/80
MAIN	974	700	FARLEIGH		
SINGLE	975	145		R25/80	80
SINGLE	975	303			100
SINGLE	976	470		80	
SINGLE	981	984		100	80
SINGLE	982	700		90	
SINGLE	984	278		80	100
SINGLE	985	857		100	R50/100
SINGLE	986	848		L50/100	100
SINGLE	992	800			80
SINGLE	994	110		100	
SINGLE	996	193		80	R25/80
MAIN	996	660	KUTTABUL		
SINGLE	997	123		L25/80	100
SINGLE	998	200		80	
SINGLE	1003	264		100	90
SINGLE	1003	781			30 (2800)
SINGLE	1003	885		30 (2800 Class)	
SINGLE	1004	963		90	100
SINGLE	1006	622		100	80
SINGLE	1007	695		90	
SINGLE	1007	964		80	R25/80
MAIN	1008	670	MT OSSA		
SINGLE	1008	850		L25/80	80
SINGLE	1011	291		80	60
SINGLE	1012	600		60	80
SINGLE	1013	722		80	90
SINGLE	1016	440		90	
SINGLE	1016	557			80
SINGLE	1018	269		100	R25/80
MAIN	1018	580	CALEN		
SINGLE	1019	210		L25/80	80
SINGLE	1020	950			100
SINGLE	1022	0		80	
SINGLE	1025	203		100	90
SINGLE	1025	738		90	80
SINGLE	1026	791		80	100
SINGLE	1031	330		100	90
SINGLE	1031	800			80
SINGLE	1033	220		90	R25/80

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MAIN	1033	600	YALBAROO		
SINGLE	1034	287		L25/80	100, T120
SINGLE	1035	868		80	
SINGLE	1037	89		100	80
SINGLE	1037	439		80	100
SINGLE	1040	377		100	70
SINGLE	1040	912		70	60
SINGLE	1041	82		60	80
SINGLE	1043	447		100	80
SINGLE	1043	712		80	60
SINGLE	1045	627		60	100
SINGLE	1047	660			80
SINGLE	1048	890		100	L25/80
MAIN	1049	350	BLOOMSBURY		
SINGLE	1049	800		R25/80	100
SINGLE	1050	860		80	
SINGLE	1052	822			T120
SINGLE	1055	257		T120	T100
SINGLE	1059	751			T135
SINGLE	1062	543		100, T135	80
SINGLE	1063	994		80	70
SINGLE	1065	310		70	90
SINGLE	1066	430			80
SINGLE	1067	460		90	L25/80
MAIN	1067	900	THOOPARA		
SINGLE	1068	362		R25/80	80
SINGLE	1068	707			100
SINGLE	1069	485		80	
MAIN	1073	200	GUNYARRA		
SINGLE	1082	960			80
SINGLE	1084	150		100	60
SINGLE	1084	269		60	L25/60
MAIN	1085	10	PROSERPINE		
SINGLE	1085	193		R25/50	60
SINGLE	1085	651		60	80
SINGLE	1086	388			90
SINGLE	1087	996			80
SINGLE	1090	554			100
SINGLE	1091	90		80	
SINGLE	1097	555	KOOLACHU		
SINGLE	1099	750			80
SINGLE	1100	969		100	L25/80
MAIN	1101	410	BUBIALO		
SINGLE	1101	854		R25/80	100
SINGLE	1102	965		80	
SINGLE	1113	0			T120
SINGLE	1114	514			80, T120
SINGLE	1115	635		100, T120	R25/80
MAIN	1116	140	LONGFORD CREEK		
MAIN	1116	440		80	60
SINGLE	1116	642		L25/60	100
SINGLE	1117	882		80	
SINGLE	1119	852			T120
SINGLE	1125	345		T120	
SINGLE	1130	685	INTABURRA		
SINGLE	1132	575		100	L25/100
MAIN	1133	100	MOOKARRA		
SINGLE	1133	624		R25/100	100
SINGLE	1145	40			70
SINGLE	1145	950		100	
SINGLE	1146	50		80	60
SINGLE	1146	250		60	70
SINGLE	1146	706		L25/70	90
SINGLE	1153	850		80	
SINGLE	1154	940			80
SINGLE	1155	970		90	L25/R25/60
MAIN	1156	580	MERINDA		
SINGLE	1156	917		60	50
SINGLE	1156	933		L25/R25	
SINGLE	1157	270		50	80
SINGLE	1158	520		R50/80	100
SINGLE	1159	450			80
SINGLE	1159	700	EURI		
SINGLE	1161	90		80	70
SINGLE	1162	25		70, T80	60, T75
SINGLE	1162	208		60, T75	80
SINGLE	1163	150		80	70
SINGLE	1164	32		70	R50/70
MAIN	1165	460	KAILI		
MAIN	1165	676		70	R25/70

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LOOP	1164	181	50	60
LOOP	1165	769	R25	L25/60
SINGLE	1165	878	L25/70	80
SINGLE	1167	830	80	R25/80
MAIN	1168	220	WATHANA	
SINGLE	1168	709	L25/80	80
SINGLE	1169	438		100
SINGLE	1169	748	80	
SINGLE	1176	122	100	80
SINGLE	1177	370	80	R25/80
MAIN	1177	950	WILMINGTON	
SINGLE	1178	240	L25/80	100
SINGLE	1179	310	80	
SINGLE	1192	445	100	80
SINGLE	1193	614	90	
SINGLE	1193	960	80	R25/80
MAIN	1194	430	GUTHALUNGRA	
SINGLE	1194	870	L25/80	100
SINGLE	1196	25	80	
SINGLE	1200	645		T120
SINGLE	1202	850	T120	
SINGLE	1209	817		80
SINGLE	1210	975	100, T120	L25/80
MAIN	1211	250	GUMLU	
SINGLE	1211	860	R25/80	90, T110
SINGLE	1213	288	80, T110	100, T120
SINGLE	1215	200	T120	
SINGLE	1220	260		T120
SINGLE	1222	267	T120	T100
SINGLE	1222	810		80
SINGLE	1224	160	100	L25/80
MAIN	1224	540	BOBAWABA	
SINGLE	1225	37	R25/80	100, T135
SINGLE	1226	375	80	
SINGLE	1228	560	T120	
SINGLE	1228	600		T120
SINGLE	1230	460	T120	
SINGLE	1234	696		80
SINGLE	1235	867	100	R25/80
MAIN	1236	290	INKERMAN	
SINGLE	1236	756	L25/80	100
SINGLE	1238	27	80	
SINGLE	1247	398		80
SINGLE	1248	500	100	
SINGLE	1248	954	80	L25/80
SINGLE	1249	219	R25/80	L25/80
MAIN	1249	330	HOME HILL	
SINGLE	1250	92	R25/80	80
SINGLE	1250	543	80	R25/80
SINGLE	1252	160	80	60
SINGLE	1253	470	60	80
SINGLE	1258	503	80	50
SINGLE	1258	738	50	70
SINGLE	1260	101	70	L25/60
MAIN	1260	860	AYR	
SINGLE	1261	301	R25/60	80
SINGLE	1261	618		80
SINGLE	1266	211	80	50
SINGLE	1266	732	50	80
SINGLE	1267	752	80	50
SINGLE	1268	63	50	80
SINGLE	1269	307	80	R25/80
MAIN	1269	750	PIONEER	
MAIN	1270	16	R25/80	
SINGLE	1270	174	L25/80	100
SINGLE	1271	332	80	
SINGLE	1274	770	100	80
SINGLE	1275	27	80	100
SINGLE	1280	745		80
SINGLE	1282	91	100	R25/80
MAIN	1282	390	BARRATTA	
SINGLE	1282	969	L25/80	100, T135
SINGLE	1284	110	80, T120	
SINGLE	1289	253		T120
SINGLE	1289	653	100, T135	80
SINGLE	1293	210	80	L25/80
MAIN	1293	610	GIRU	
SINGLE	1294	209	R25/80	80
SINGLE	1300	336	80	R25/80
MAIN	1300	930	CROMARTY	

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SINGLE	1301	239	L25/80	90
SINGLE	1302	151		100
SINGLE	1302	250	80	
SINGLE	1304	109	100	90
SINGLE	1304	402	90	100
SINGLE	1306	65		80
SINGLE	1307	183	100, T120	L25/80
MAIN	1307	596	STORTH	
SINGLE	1308	90	R25/80	100
SINGLE	1309	839		80
SINGLE	1310	469	80	60
SINGLE	1310	905	60	100
SINGLE	1312	621	100	90
SINGLE	1315	123	90	100
SINGLE	1316	920	T100	T120
SINGLE	1319	940		80
SINGLE	1320	312	100, T120	
SINGLE	1321	206		80
DOWN	1321	310	NOME	
DOWN	1327	664	80	70
DOWN	1328	27	70	80
DOWN	1328	601	L25/80	L25/80
DOWN	1330	45		50
DOWN	1330	172	R25/80	R50/50
DOWN	1330	426	L50	
DOWN	1330	775	50	
DOWN	1330	879		L25
DOWN	1331	70	STUART	
DOWN	1331	374	L50	
DOWN	1331	505	L50	
DOWN	1331	683	50	R50
DOWN	1331	812		80
DOWN	1332	355	50	
DOWN	1334	200	L50/80	80
DOWN	1338	155	80	40
DOWN	1339	910	40	25
UP	1321	310	NOME	
UP	1327	150	80	R25/80
UP	1327	556		R25
UP	1327	574	L25/80	
UP	1327	659	80	70
UP	1328	200	L50/70	70
UP	1328	498	70	L25/80
UP	1329	351	80	R15
UP	1330	45		50
UP	1330	296	R50/80	L50
UP	1331	70	STUART	
UP	1331	318		L50
UP	1331	812	R50/50	R25/80
UP	1332	355	50	
UP	1334	74	L50/80	L50/80
UP	1338	155	80	40
UP	1339	874	40	
UP	1339	910		25
SINGLE	1340	45	25	40
SINGLE	1340	330	R40/40	L25
MAIN	1340	877	L25	
LOOP			TOWNSVILLE STATION	
SINGLE	1340	988	R25/40	40
SINGLE	1344	215	40	40
SINGLE	1344	942	50	R25/60
MAIN	1345	395		L25/60
SINGLE	1345	930	60/25L	80
SINGLE	1348	400		100
SINGLE	1348	760	80	
SINGLE	1352	178	100	
SINGLE	1353	12	80	90
SINGLE	1354	938		80
SINGLE	1355	924	100	
SINGLE	1355	982		100/50L
MAIN	1356	480	DEERAGUN	
SINGLE	1357	136	R50/80	100
SINGLE	1358	127	80	
SINGLE	1364	32		80
SINGLE	1364	850	100	
SINGLE	1364	980	YABULU	
SINGLE	1365	53		R25/80
SINGLE	1365	670		R25/80

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SINGLE	1367	693		80	25
DN MAIN	1368	60	PURONO		
SINGLE	1368	714		25	80
SINGLE	1378	840		80	
SINGLE	1378	865			L25/70
MAIN	1379	410	KURUKAN		
SINGLE	1379	701			
SINGLE	1379	801			80
SINGLE	1384	800		80	100
SINGLE	1390	0		100	80
SINGLE	1393	802		80	
SINGLE	1393	913			L25/70
MAIN	1394	170	ROLLINGSTONE		
SINGLE	1394	740			
SINGLE	1394	839			80
SINGLE	1408	603		80	
SINGLE	1408	671			R25/70
MAIN	1409	310	MUTARNEE		
SINGLE	1409	524			
SINGLE	1409	644			80
SINGLE	1423	462		80	
SINGLE	1423	504			R25/70
MAIN	1423	647	BAMBAROO		
SINGLE	1424	380		L25/70	80
SINGLE	1435	786		80	
SINGLE	1435	853			R25/70
MAIN	1436	330	POMBEL		
MAIN	1436	640			25
LOOP	1436	640			25
SINGLE	1436	714			
SINGLE	1436	813			40
SINGLE	1437	115		40	80
SINGLE	1448	962			50
SINGLE	1449	200		80	25
MAIN	1449	300		12	
MAIN	1449	436	INGHAM		
SINGLE	1450	201		25	
SINGLE	1450	205			80
SINGLE	1451	201		80	70
SINGLE	1451	989		70	40
SINGLE	1452	172		40	80
SINGLE	1457	565		80	40
SINGLE	1457	646		40	80
SINGLE	1459	744		80	50
SINGLE	1459	920	BEMERSIDE		
SINGLE	1460	245		50	60
SINGLE	1461	157		60	40
SINGLE	1462	398		60	80
SINGLE	1466	231		80	R25/70
MAIN	1466	588	HINCHINBROOK		
SINGLE	1467	109			80
SINGLE	1479	692		80	R25/70
MAIN	1480	104	CONN		
SINGLE	1480	710			80
SINGLE	1494	154		80	70
SINGLE	1496	816		70	80
SINGLE	1500	416		80	70
SINGLE	1501	684		70	
SINGLE	1501	689			L25/60
MAIN	1502	510	CARDWELL		
SINGLE	1502	716			80
SINGLE	1507	388		80	60

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TRACK	DISTANCE km metres		FEATURE	EXISTING SPEED BOARDS	
				UP TRAIN	DOWN TRAIN
				TO BRISBANE	TO CAIRNS
SINGLE	1508	137		60	70
SINGLE	1508	763		70	80
SINGLE	1511	963		80	L25/70
SINGLE	1512	960		R25/70	70
SINGLE	1512	960	KENNEDY		
SINGLE	1514	789		70	80
SINGLE	1523	725		80	70
SINGLE	1524	150	BILYANA		
SINGLE	1525	171		L25/70	80
SINGLE	1537	194		80	40
SINGLE	1537	314		40	80
SINGLE	1538	777		80	40
SINGLE	1539	354		40	80
SINGLE	1541	565		80	50
SINGLE	1542	71		50	25
SINGLE	1542	673		25	50
SINGLE	1543	264		50	80
SINGLE	1544	831		80	70
SINGLE	1545	129		70	
SINGLE	1545	356			L25/70
MAIN	1545	610	TULLY		
LOOP	1545	450		12	
SINGLE	1546	30		R25/70	
SINGLE	1546	483		70	80
SINGLE	1552	716		80	40
SINGLE	1552	865		40	80
SINGLE	1555	126		80	50
SINGLE	1555	425		50	60
SINGLE	1555	696		60	70
SINGLE	1557	762		70	50
SINGLE	1558	716		50	
SINGLE	1558	740			80
SINGLE	1560	47		80	40
SINGLE	1560	147		40	80
SINGLE	1560	732		80	
SINGLE	1561	309			L25/70
MAIN	1561	480	EL-ARISH		
SINGLE	1562	150		R25/70	
SINGLE	1562	329			80
SINGLE	1564	511		80	40
SINGLE	1564	615		40	80
SINGLE	1567	871		80	R25/70
MAIN	1568	740	SILKWOOD		
MAIN	1568	703			10
MAIN	1568	769		70	
LOOP	1568	702			10
SINGLE	1568	880		10	80
SINGLE	1575	415		80	50
SINGLE	1576	390		50	80
SINGLE	1577	551		80	50
SINGLE	1578	621		50	80
SINGLE	1581	749		80	40
SINGLE	1582	80		40	80
SINGLE	1584	65		80	L25/70
MAIN	1584	900			20
MAIN	1584	910	BOOGAN		
LOOP	1584	901			20
SINGLE	1584	958		70	
SINGLE	1585	55		20	80
SINGLE	1586	588		80	60
SINGLE	1587	2		60	80

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SINGLE 1588 885			80	40
			EXISTING	
			UP TRAIN TO	DOWN TRAIN TO CAIRNS
SINGLE	1588	975	40	80
SINGLE	1589	737	80	40
SINGLE	1589	827	40	80
SINGLE	1592	823	80	60
SINGLE	1593	433	60	25
MAIN	1594	120	INNISFAIL	
SINGLE	1594	382	25	80
SINGLE	1594	919	50	
SINGLE	1596	159	80	70
SINGLE	1596	354	70	40
SINGLE	1596	434	40	80
SINGLE	1600	42	80	40
SINGLE	1600	140	40	60
SINGLE	1601	622	60	25
SINGLE	1601	751	25	60
SINGLE	1602	561		40
SINGLE	1602	974	60	80
SINGLE	1603	730	80	60
SINGLE	1604	87	60	80
SINGLE	1604	598	80	60
SINGLE	1605	174	60	80
SINGLE	1605	688	80	60
SINGLE	1606	291	60	50
SINGLE	1607	441	50	R25/70
MAIN	1608	130	WAUGH	
SINGLE	1608	229	L25/70	80
SINGLE	1609	576	80	70
SINGLE	1609	872	70	50
SINGLE	1610	98	50	80
SINGLE	1610	939	80	70
SINGLE	1611	301	70	60
SINGLE	1611	467	60	70
SINGLE	1612	955	70	80
SINGLE	1614	798	80	40
SINGLE	1614	882	40	80
SINGLE	1618	552	80	50
SINGLE	1618	805	50	80
SINGLE	1619	835	80	40
SINGLE	1620	181	40	80
SINGLE	1621	106	80	40
SINGLE	1621	433	40	50
SINGLE	1622	165	50	L25/50
MAIN	1622	290	BABINDA	
SINGLE	1622	971	R25/50	60
SINGLE	1623	123	60	80
SINGLE	1623	407	80	40
SINGLE	1623	500	40	80
SINGLE	1625	930	80	40
SINGLE	1626	218	40	60
SINGLE	1626	454	60	80
SINGLE	1627	218	80	70
SINGLE	1627	667	70	50
SINGLE	1627	882	50	80
SINGLE	1628	896	80	60
SINGLE	1629	104	60	80
SINGLE	1629	674	80	40
SINGLE	1629	775	40	60
SINGLE	1630	448	60	40
SINGLE	1630	702	40	80
SINGLE	1632	194	80	40

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SINGLE	1632	593		80
SINGLE	1632	654	40	
SINGLE	1633	667	80	50
SINGLE	1634	400	50	80
SINGLE	1636	35	80	R25/70
MAIN	1636	520	DEERAL	
SINGLE	1636	899	L25/70	80
SINGLE	1642	744	80	60
SINGLE	1643	707	60	80
SINGLE	1645	655	80	50
SINGLE	1646	277	50	80
MAIN	1651	604	80	70/25R
MAIN	1652	310	ALOOMBA	
MAIN	1652	400	70	40
SINGLE	1652	647	25	80
SINGLE	1653	370	80	40
SINGLE	1653	439	40	50
SINGLE	1653	679	50	80
SINGLE	1654	968	80	50
SINGLE	1655	335	50	60
SINGLE	1655	762	60	80
SINGLE	1656	740	80	40
SINGLE	1658	171		R25
MAIN	1658	350	GORDONVALE	
MAIN	1658	490	L25	
SINGLE	1659	171	40	80
SINGLE	1659	922	80	40
SINGLE	1660	127	40	60
SINGLE	1661	915	60	80
SINGLE	1663	73	80	L25/70
SINGLE	1663	963	R25/70	80
SINGLE	1664	310	80	60
SINGLE	1664	455	60	80
SINGLE	1665	0	80	50
SINGLE	1665	273	50	80
SINGLE	1668	530	EDMONTON	
SINGLE	1670	114	80	60
SINGLE	1670	598	60	50
SINGLE	1671	13	50	80
SINGLE	1671	765	80	60
SINGLE	1671	927	60	80
SINGLE	1673	203	80	40
SINGLE	1673	908	40	50
SINGLE	1674	862	50	R25/80
SINGLE	1676	66	L25/80	
SINGLE	1677	79	80	L25/80
MAIN	1678	354	80	60
SINGLE	1678	503	R25	60
SINGLE	1679	255	60	30
SINGLE	1679	603	30	60
SINGLE	1680	67	60	20
SINGLE	1680	400	20	
MAIN	1680	1005	CAIRNS	
SINGLE	1681	320	25	20

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TRACK	DISTANCE km	FEATURE	EXISTING SPEED BOARDS	
			UP TRAIN TO R'PTON	DOWN TRAIN TO YEPPOON
SINGLE	0.000	GLENMORE JUNCTION		
SINGLE	0.195		25	25
DN MAIN	1.530	NORTH ROCKHAMPTON		
SINGLE	2.070		25	60
SINGLE	5.875		60	R25/40
SINGLE	6.383			R25
MAIN	6.680	LAKES CREEK		
SINGLE	6.920		L25/40	60
SINGLE	7.150		50	
SINGLE	9.430	NERIMBERA		
SINGLE	10.711			40
SINGLE	10.900		60	
MAIN	11.070	FIELDS SIDING		
SINGLE	11.190		25	
SINGLE	11.420			60
SINGLE	12.800		60	
SINGLE	12.910		20	
	13.750	TRACK TO BE REMOVED PAST THIS POINT		
SINGLE	13.880		60	50
MAIN	21.270	SLEIPNER		
SINGLE	21.380		50	30
SINGLE	21.510		30	40
SINGLE	22.455		40	30
SINGLE	22.730			50
SINGLE	23.730		40	30
SINGLE	24.285		30	40
SINGLE	24.500		40	50
SINGLE	26.480		50	R25/50
MAIN	27.200	MOUNT CHALMERS		
SINGLE	27.240		L25/50	40
SINGLE	31.165		40	50
SINGLE	34.570	CAWARRAL		
SINGLE	44.920	BONDOOLA		
SINGLE	52.350		40	30
SINGLE	53.100		30	15
MAIN	53.530	YEPPOON		

TRACK	DISTANCE km	FEATURE	EXISTING SPEED BOARDS	
			UP TRAIN TO ERAKALA	DOWN TRAIN TO HARBOUR
SINGLE	970.003		80	R50/80
		MACKAY HARBOUR LINE		
SINGLE	0.820		R25/50	80
SINGLE	1.346		80	L25/80
DN MAIN	2.146			25
SINGLE	2.240		25	70p
SINGLE	2.675			80
SINGLE	3.794		70	
SINGLE	5.700		80	70
SINGLE	6.796		70	60
SINGLE	7.200		60	70
SINGLE	9.039		70	L25/50
MAIN	10.153		50	L15/25
LOOP	10.256		L15/25	25

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MACKAY to MARIAN

Location	Up Train	Down Train
km metres	to Marian	to Mackay
0 450	25	25
1 488	50	25
2 250	25	50
3 100	50	25
9 200	25	50
10 220	50	25
22 850	25	50

BOWEN COKEWORKS

Up Train to Proserpine Down Train to
 Cokeworks No speed boards (**observe Yard
 Speed 25 km/h**)

TOWNSVILLE JETTY

Up Train to Townsville Down Train
 to Jetty No speed boards (**observe Yard**

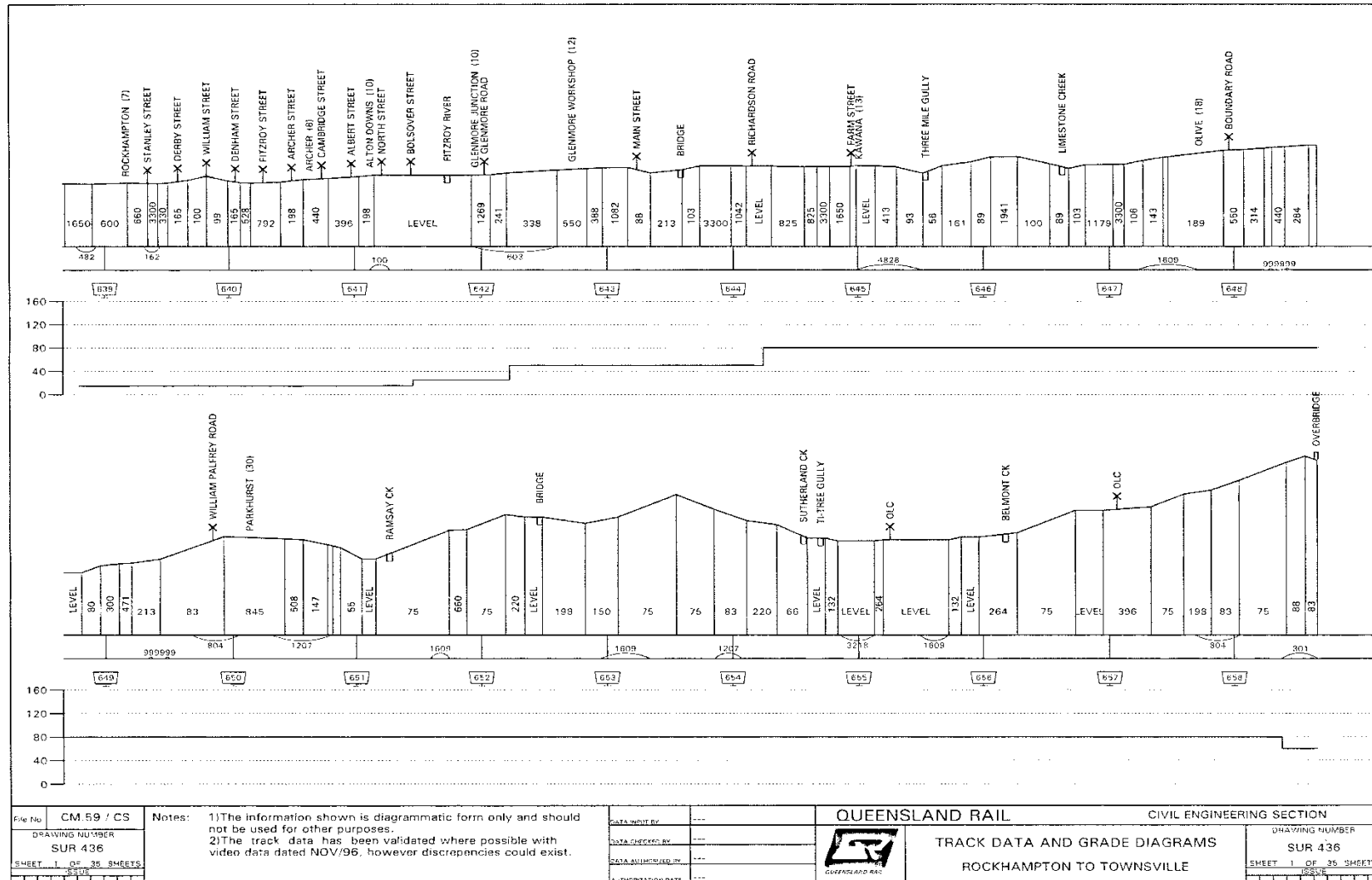
Speed 25 km/h) COBARRA LOOP

No speed boards (**observe Yard Speed 25 km/h**)

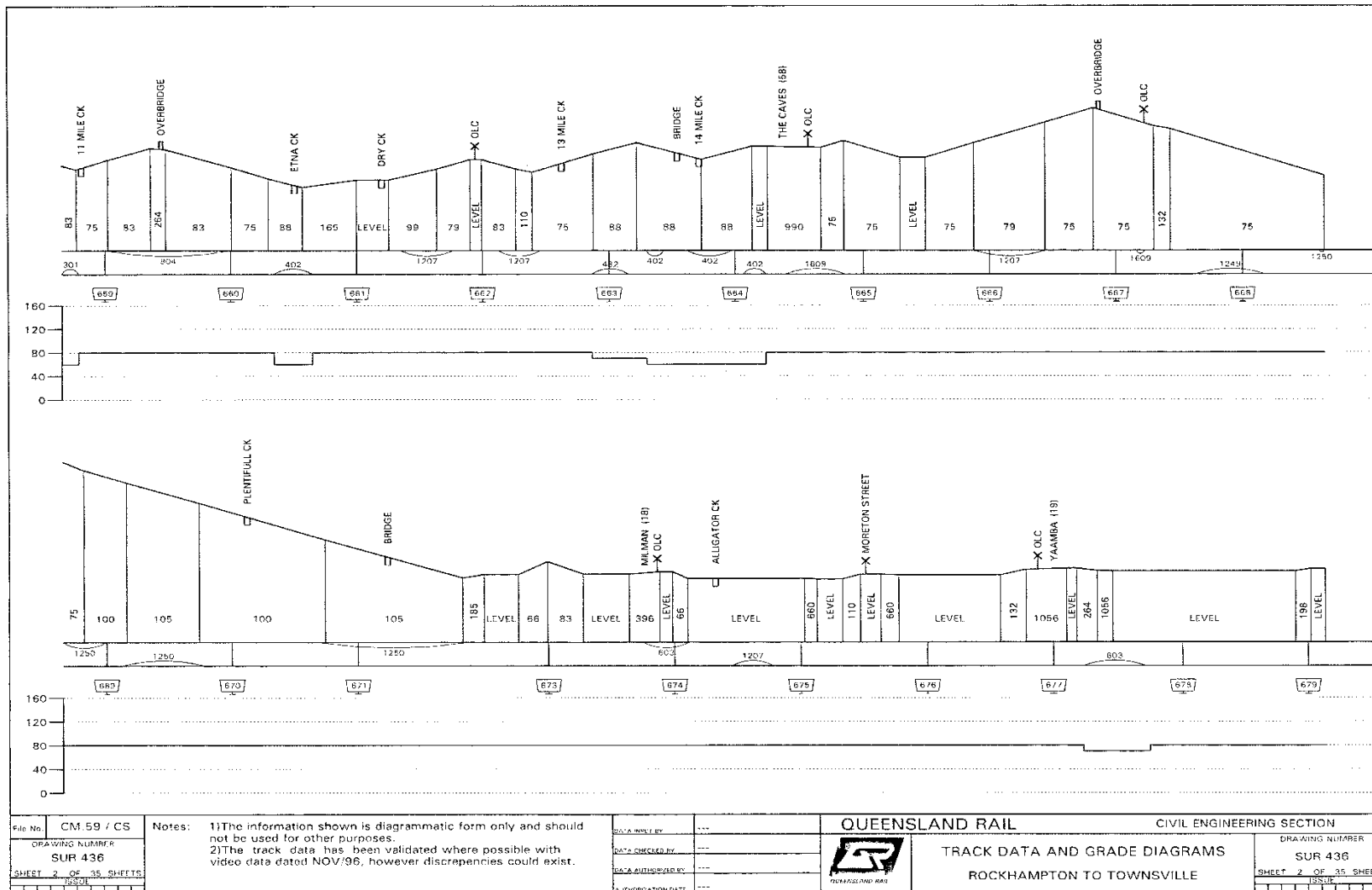
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APPENDIX E

Track Data & Grade Diagrams



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File No.	CM 59 / CS
OPERATING NUMBER	SUR 436
SHEET 2 OF 35 SHEETS	

Notes: 1)The information shown is diagrammatic form only and should not be used for other purposes.
2)The track data has been validated where possible with video data dated NOV/96, however discrepancies could exist.

SCALE	---
DATE CHECKED BY	---
SCALE AUTHORITY	---
DATE	---

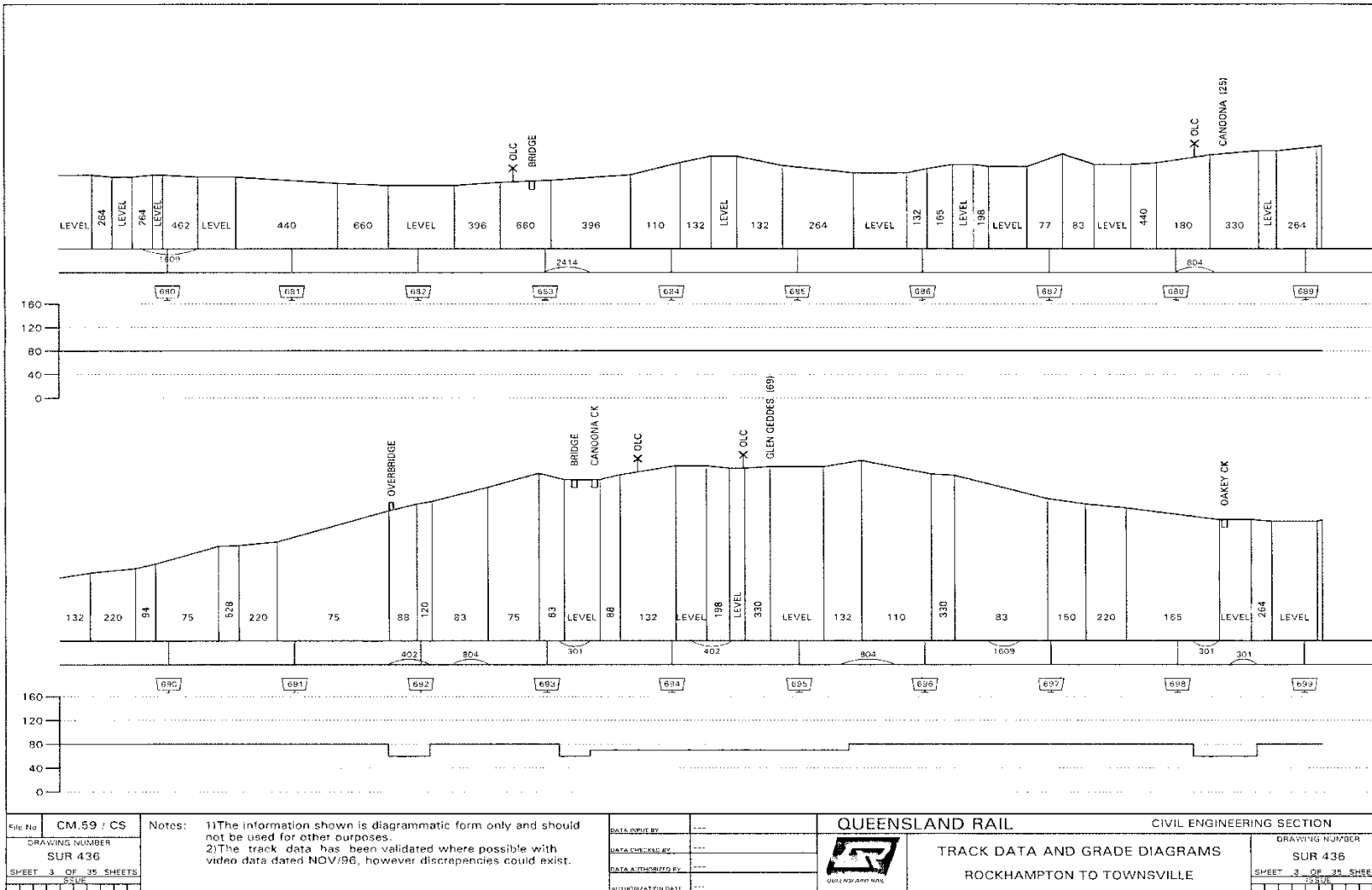
QUEENSLAND RAIL CIVIL ENGINEERING SECTION



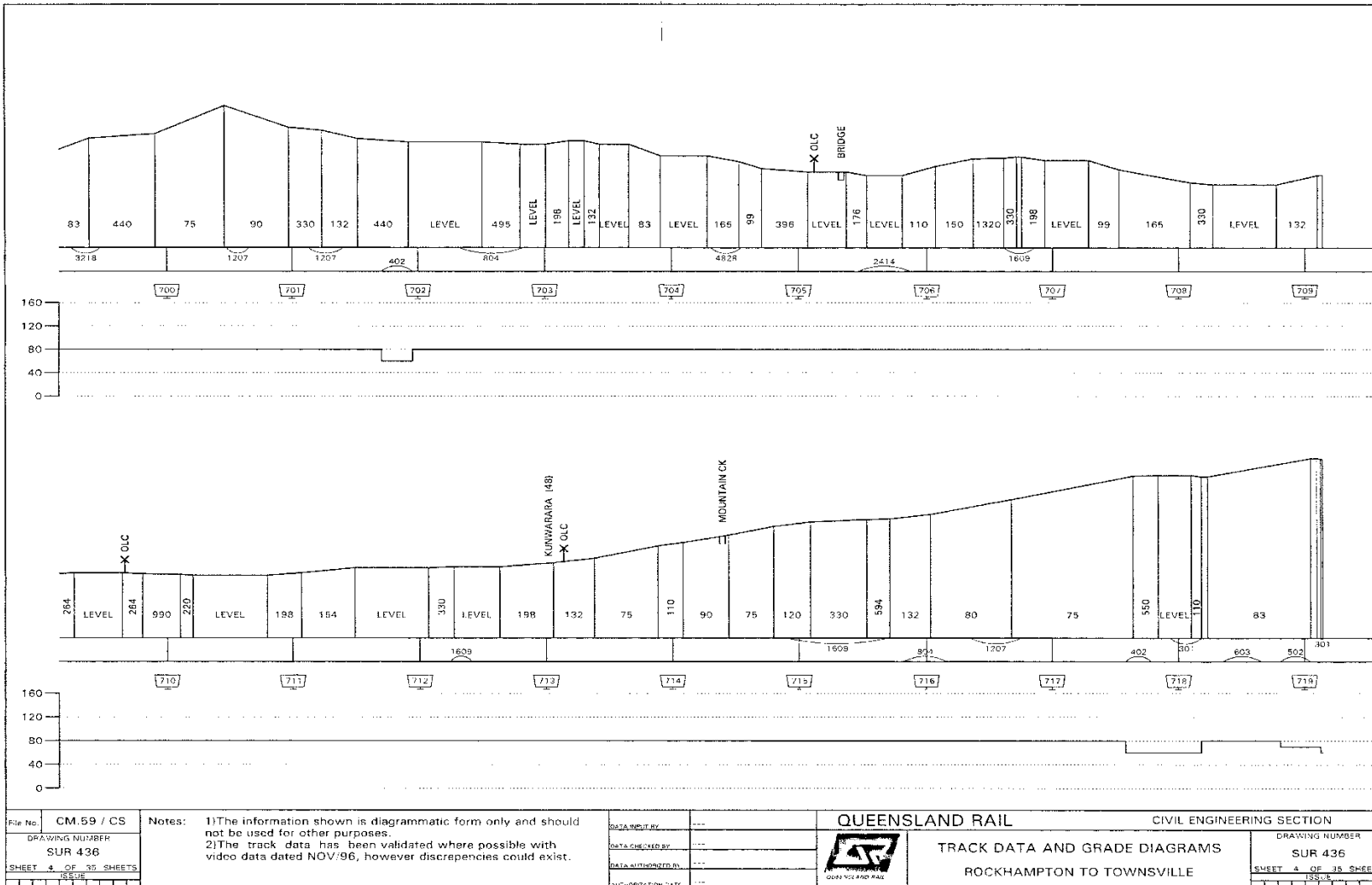
TRACK DATA AND GRADE DIAGRAMS
ROCKHAMPTON TO TOWNVILLE

DRAWING NUMBER	SUR 436
SHEET 2 OF 35 SHEETS	

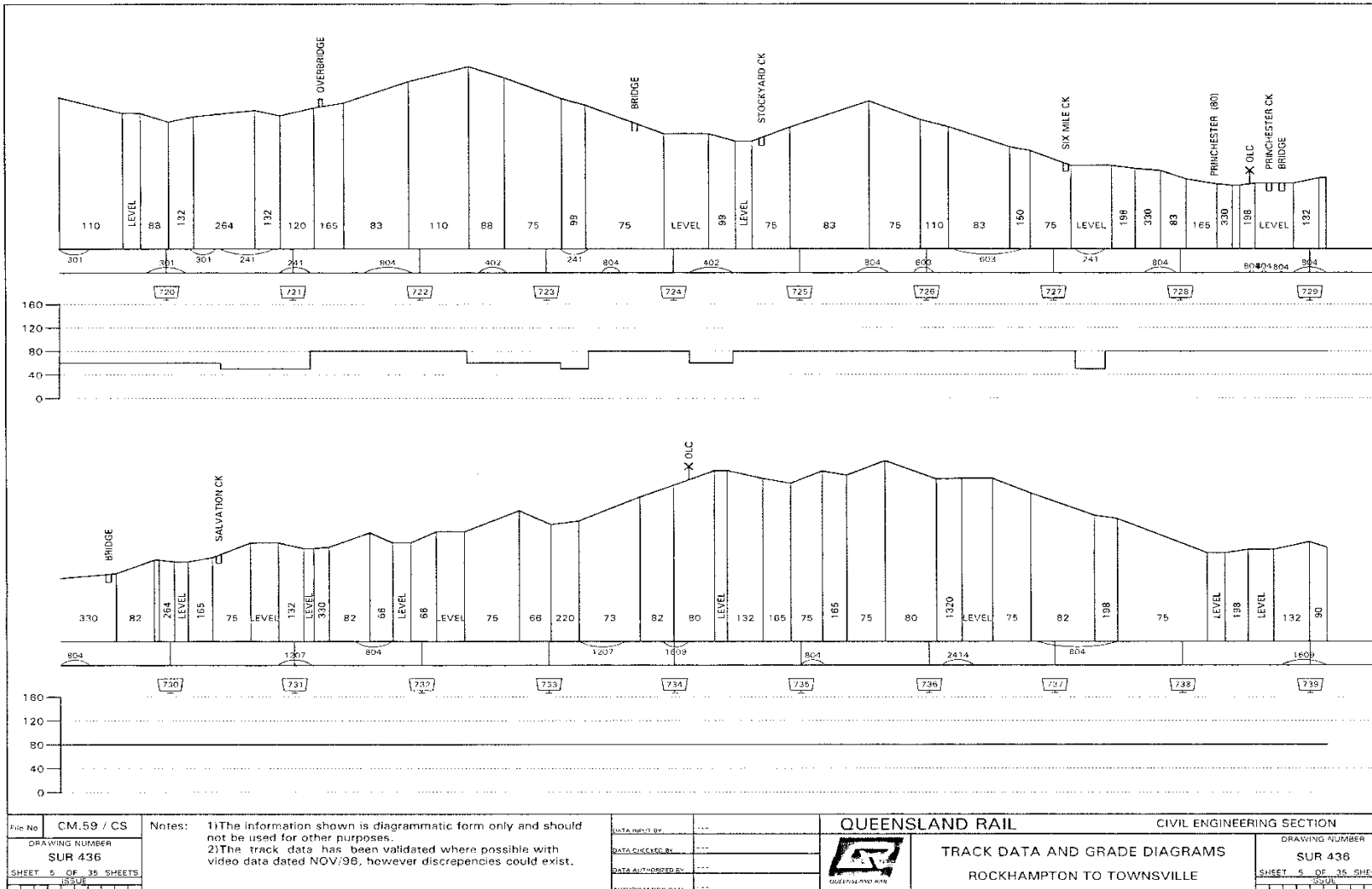
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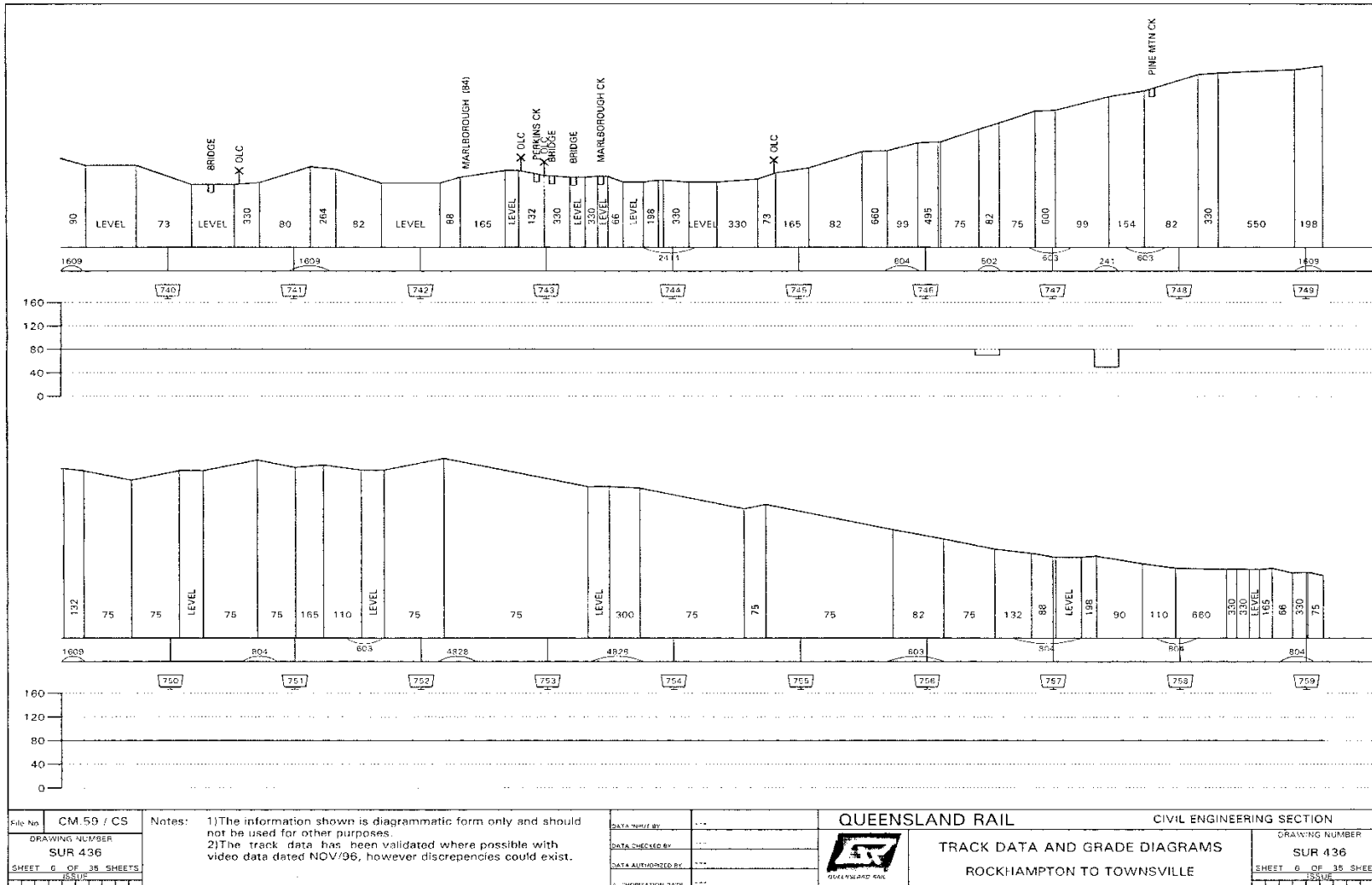
File No: CM.59 / CS
 DRAWING NUMBER: SUR 436
 SHEET 5 OF 35 SHEETS
 ISSUE

Notes: 1) The information shown is diagrammatic form only and should not be used for other purposes.
 2) The track data has been validated where possible with video data dated NOV/96, however discrepancies could exist.

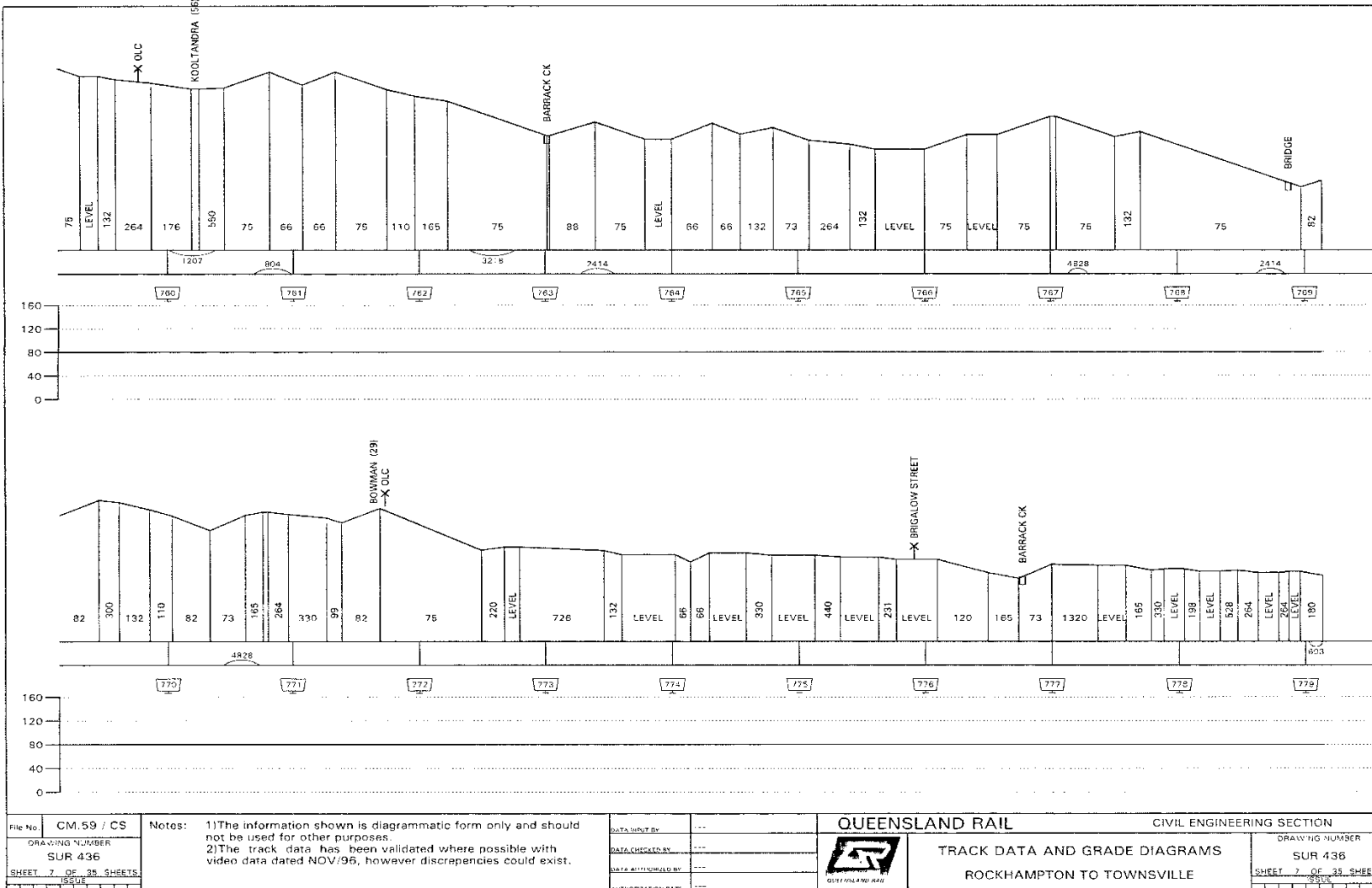
DATA INPUT BY: ---
 DATA CHECKED BY: ---
 DATA AUTHORIZED BY: ---
 AUTHORITY & ISSUE DATE: ---


QUEENSLAND RAIL		CIVIL ENGINEERING SECTION
		DRAWING NUMBER
TRACK DATA AND GRADE DIAGRAMS		SUR 436
ROCKHAMPTON TO TOWNVILLE		SHEET 5 OF 35 SHEETS
		ISSUE

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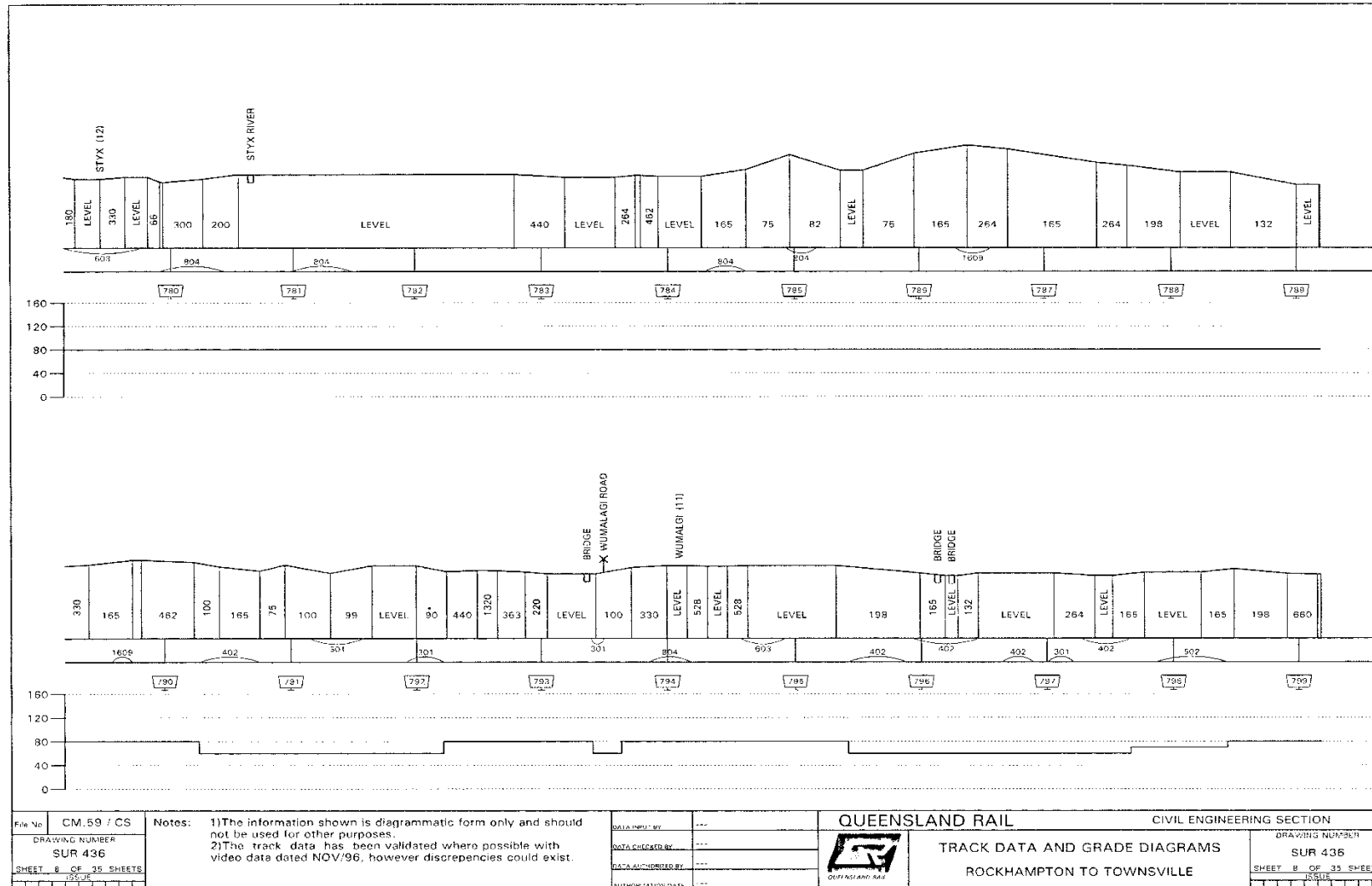


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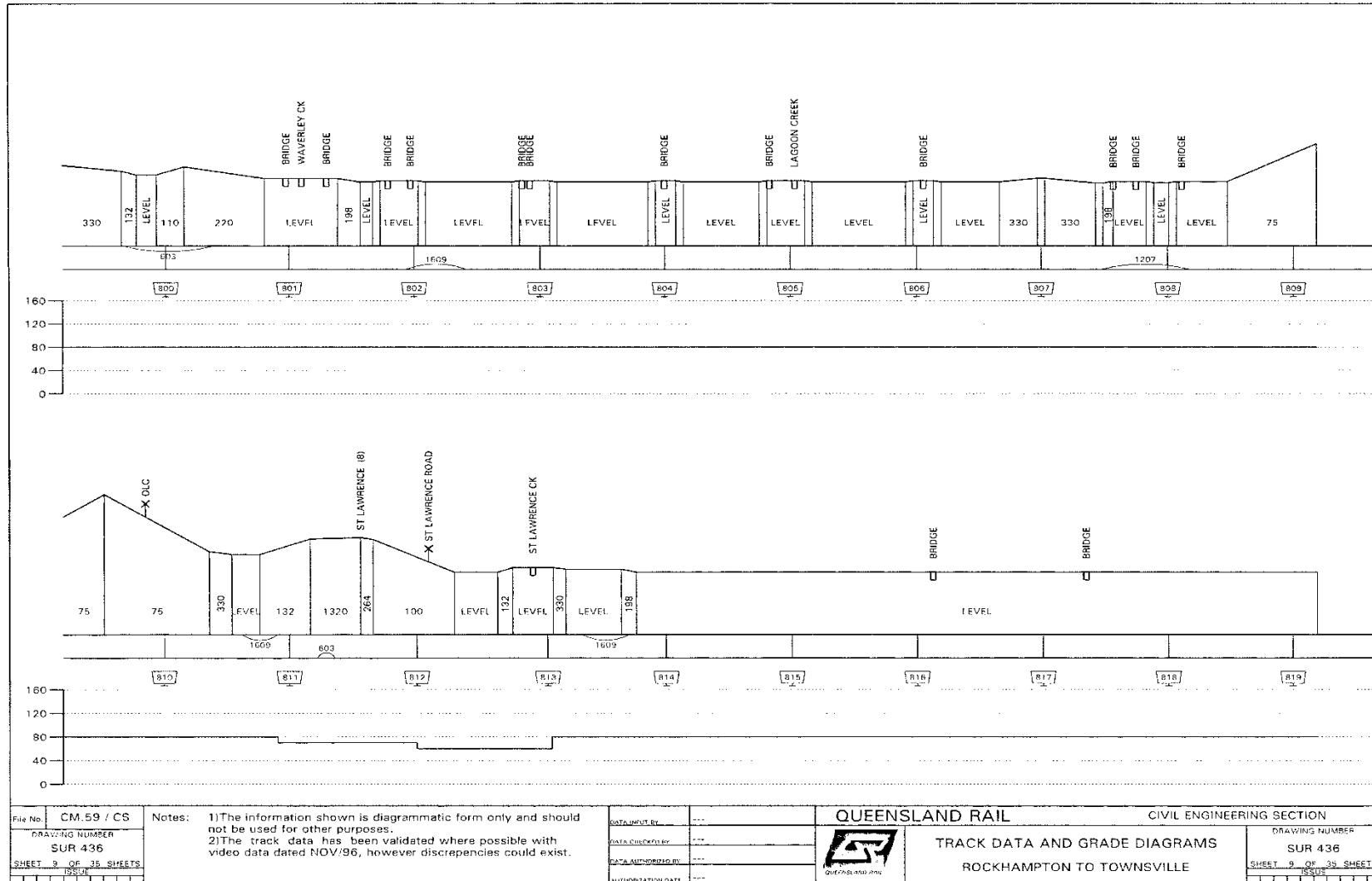


File No. CM.59 / CS	Notes: 1)The information shown is diagrammatic form only and should not be used for other purposes. 2)The track data has been validated where possible with video data dated NOV/96, however discrepancies could exist.	DATA INPUT BY: ---	 QUEENSLAND RAIL CIVIL ENGINEERING SECTION TRACK DATA AND GRADE DIAGRAMS ROCKHAMPTON TO TOWNVILLE	DRAWING NUMBER SUR 436
DRAWING NUMBER SUR 436		DATA CHECKED BY: ---		DRAWING NUMBER SUR 436
SHEET 7 OF 35 SHEETS		DATE APPROVED BY: ---		SHEET 7 OF 35 SHEETS
ISSUE		AUTHORISATION DATE: ---		ISSUE

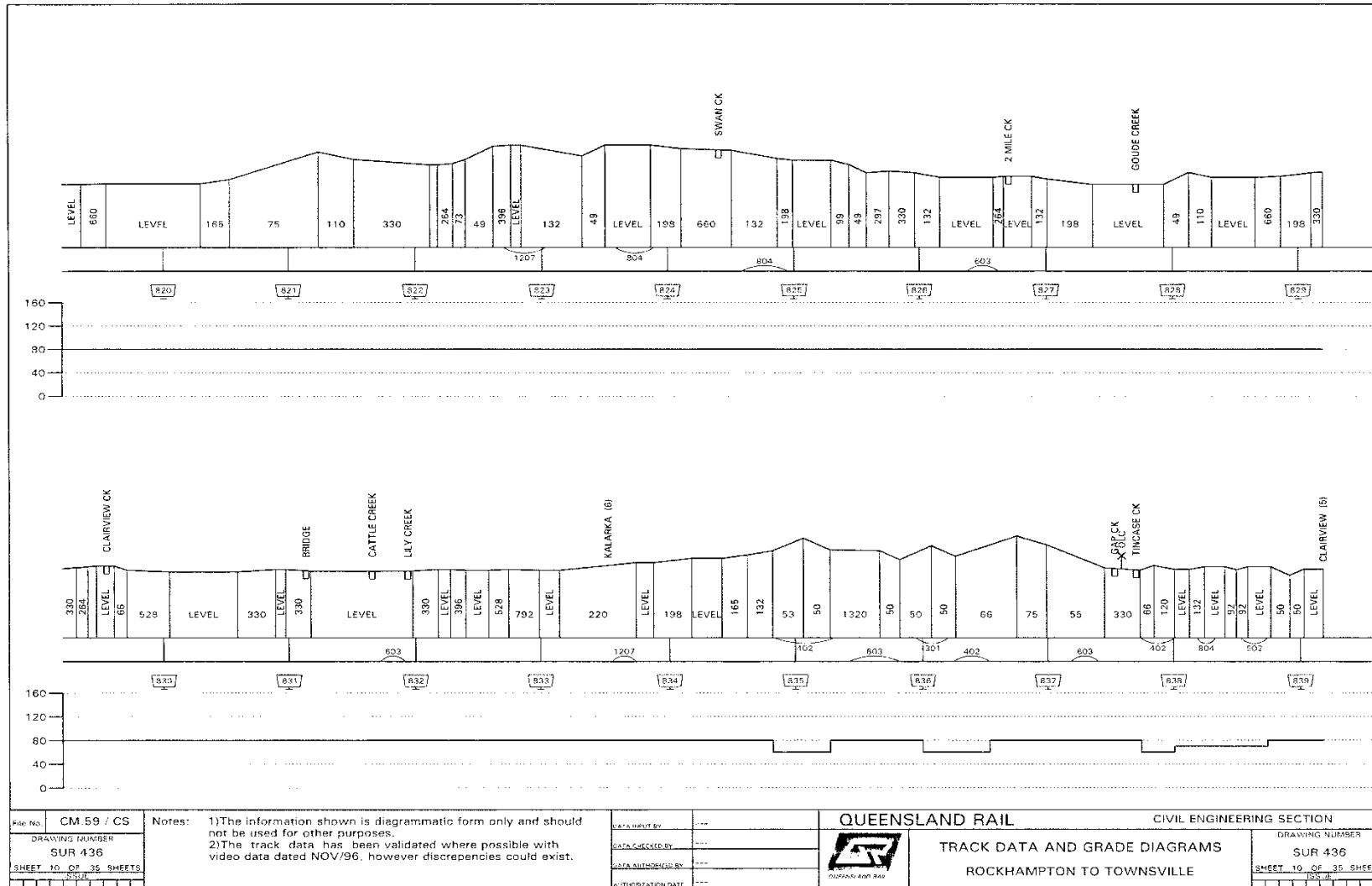
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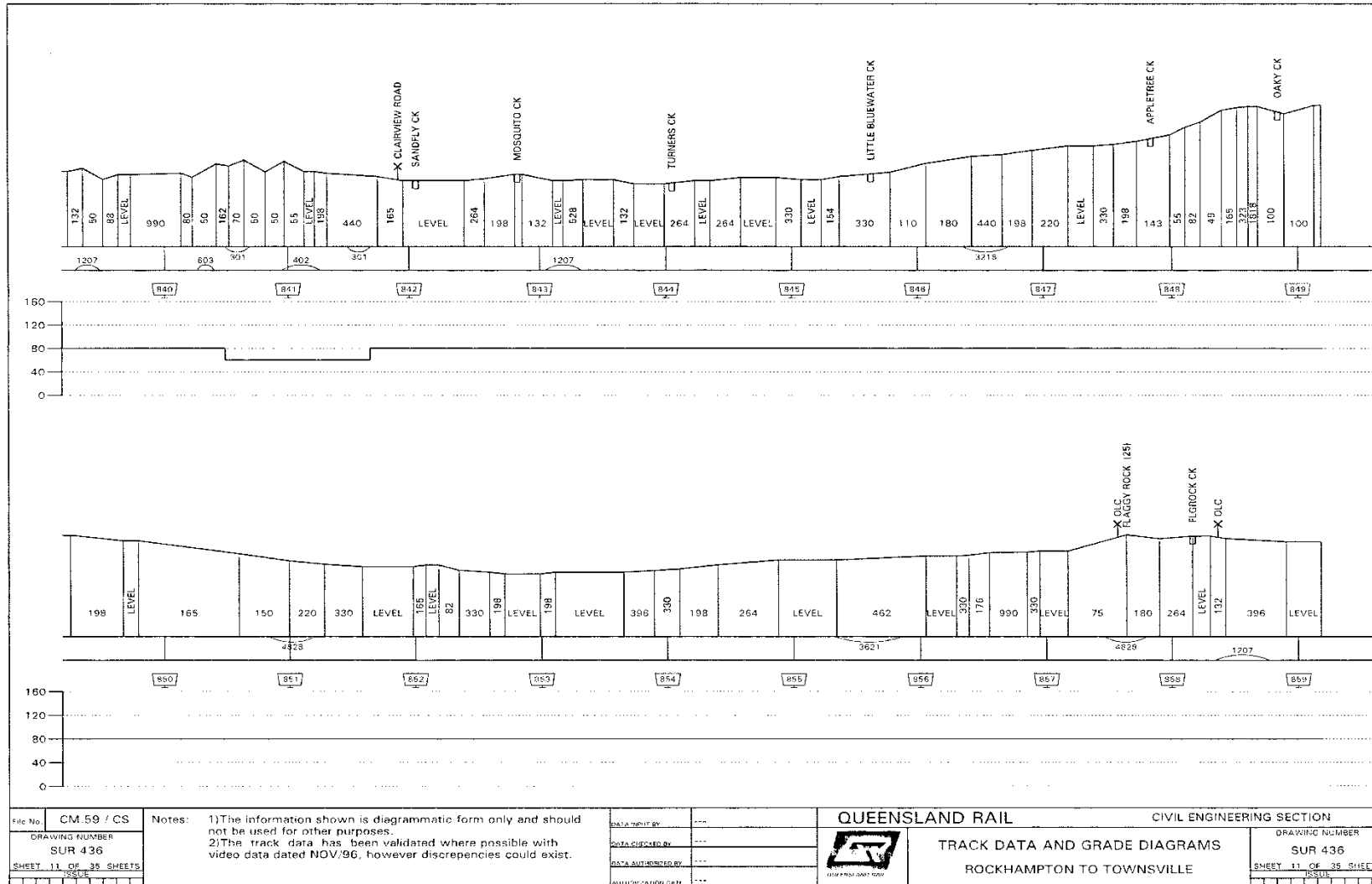
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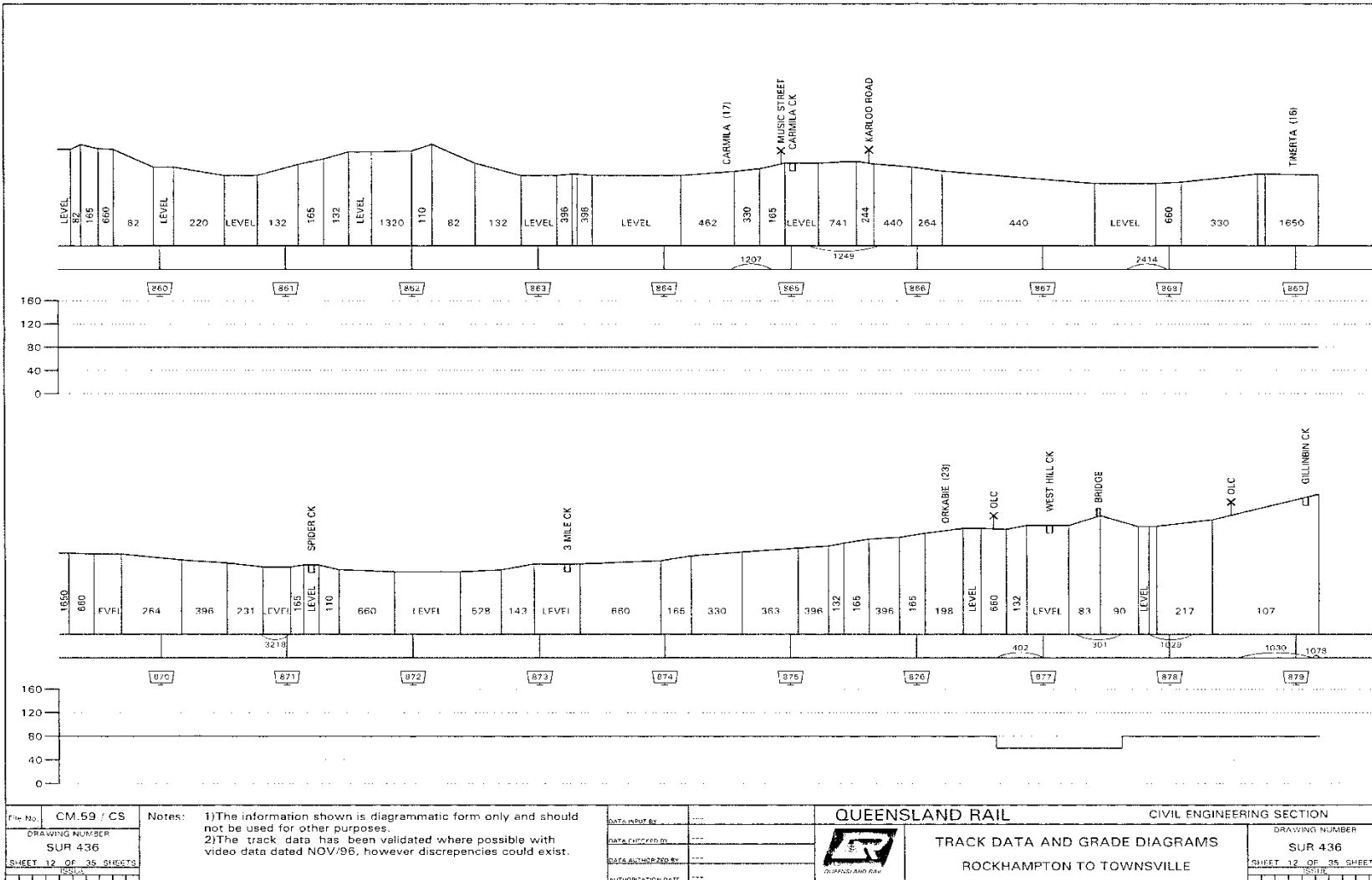
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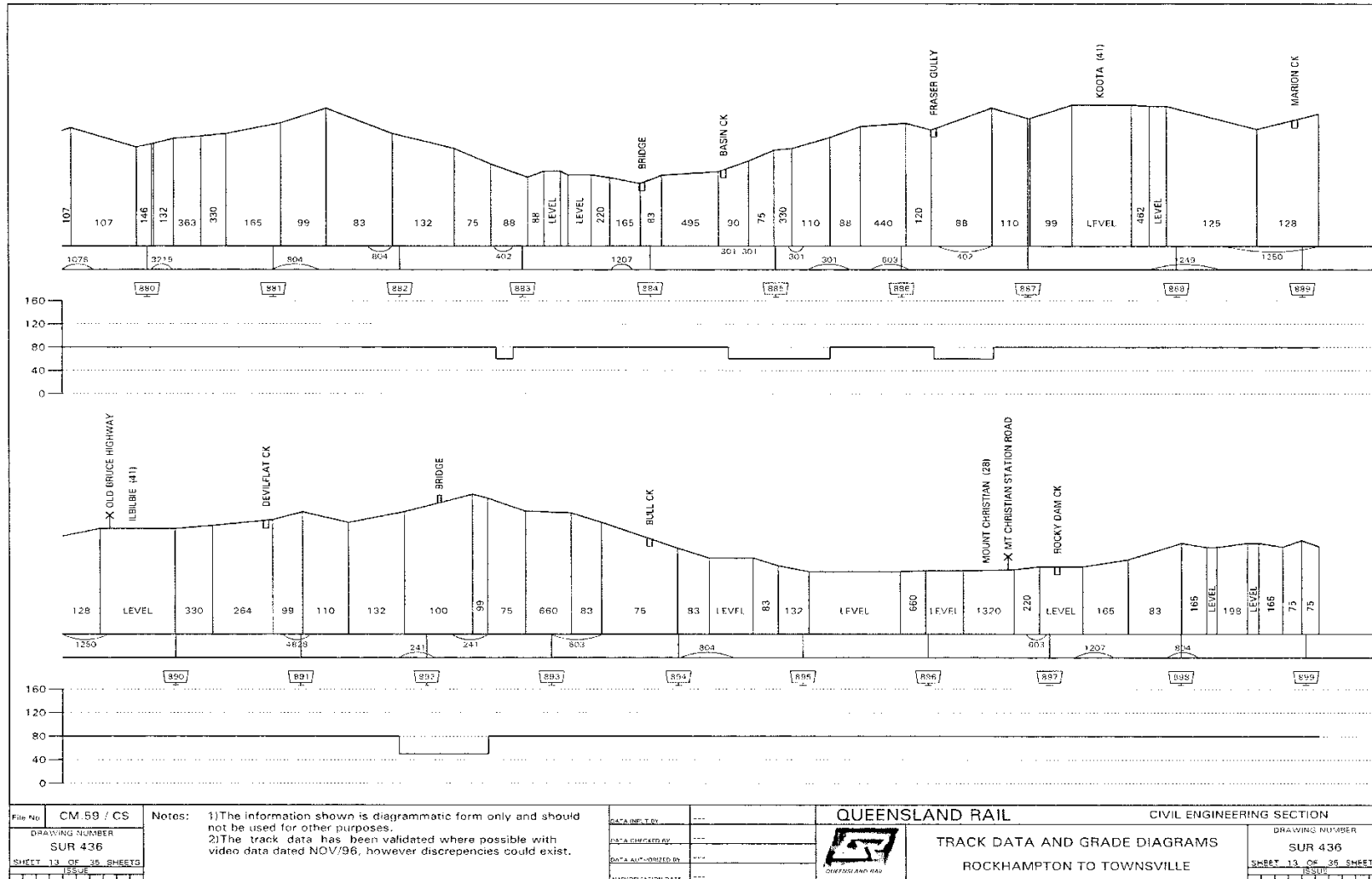
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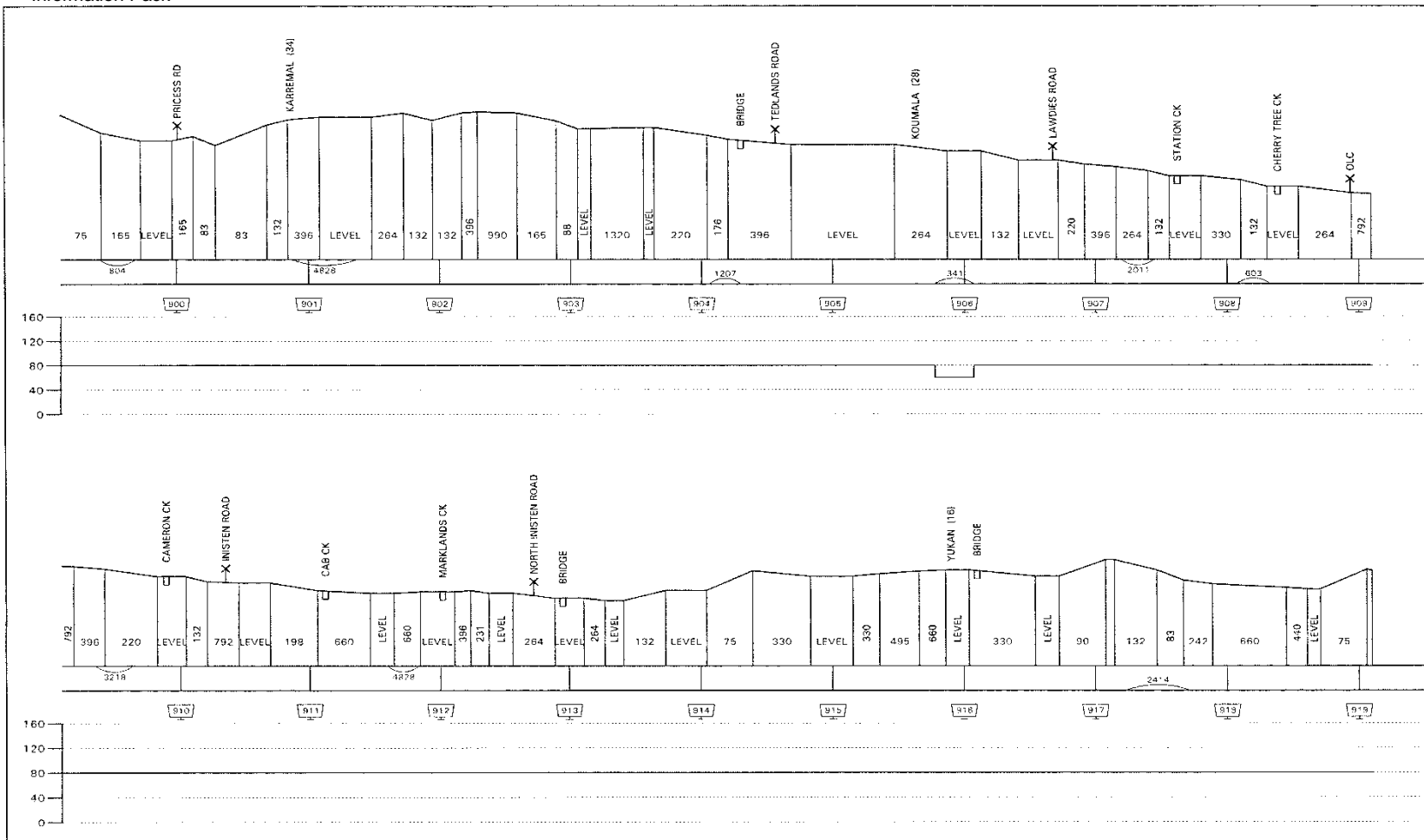
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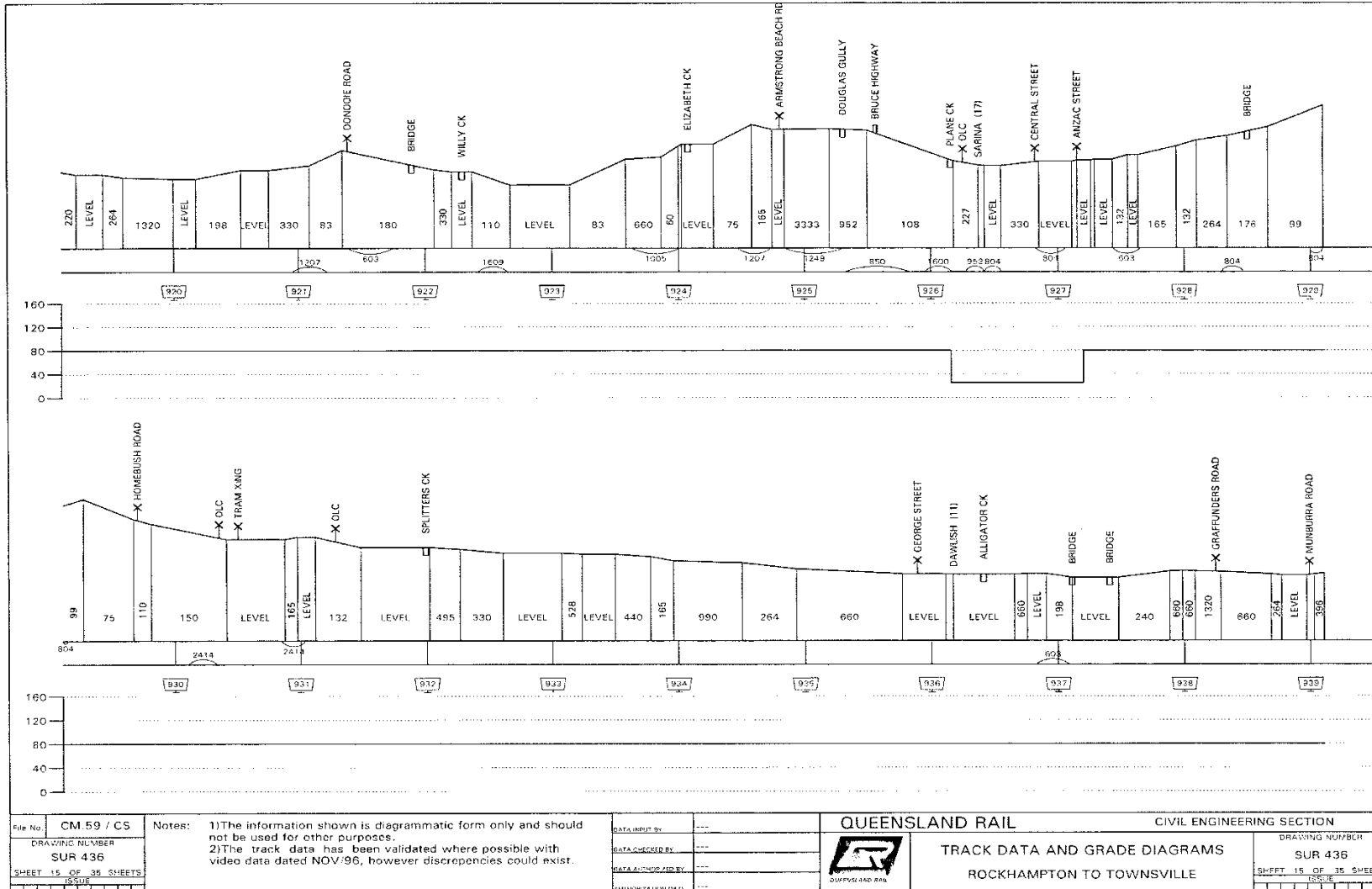


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File No: CM.59 / CS	Notes: 1)The information shown is diagrammatic form only and should not be used for other purposes. 2)The track data has been validated where possible with video data dated NOV/96, however discrepancies could exist.	DATA INPUT BY: ---		QUEENSLAND RAIL	CIVIL ENGINEERING SECTION
DRAWING NUMBER: SUR 436		DATA CHECKED BY: ---		TRACK DATA AND GRADE DIAGRAMS	DRAWING NUMBER: SUR 436
SHEET 14 OF 35 SHEETS		DATA AUTHORIZED BY: ---		ROCKHAMPTON TO TOWNSVILLE	SHEET 14 OF 35 SHEETS
ISSUE		AUTHORIZATION DATE: ---			ISSUE

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DRAWING NUMBER: SUR 436
SHEET 15 OF 35 SHEETS

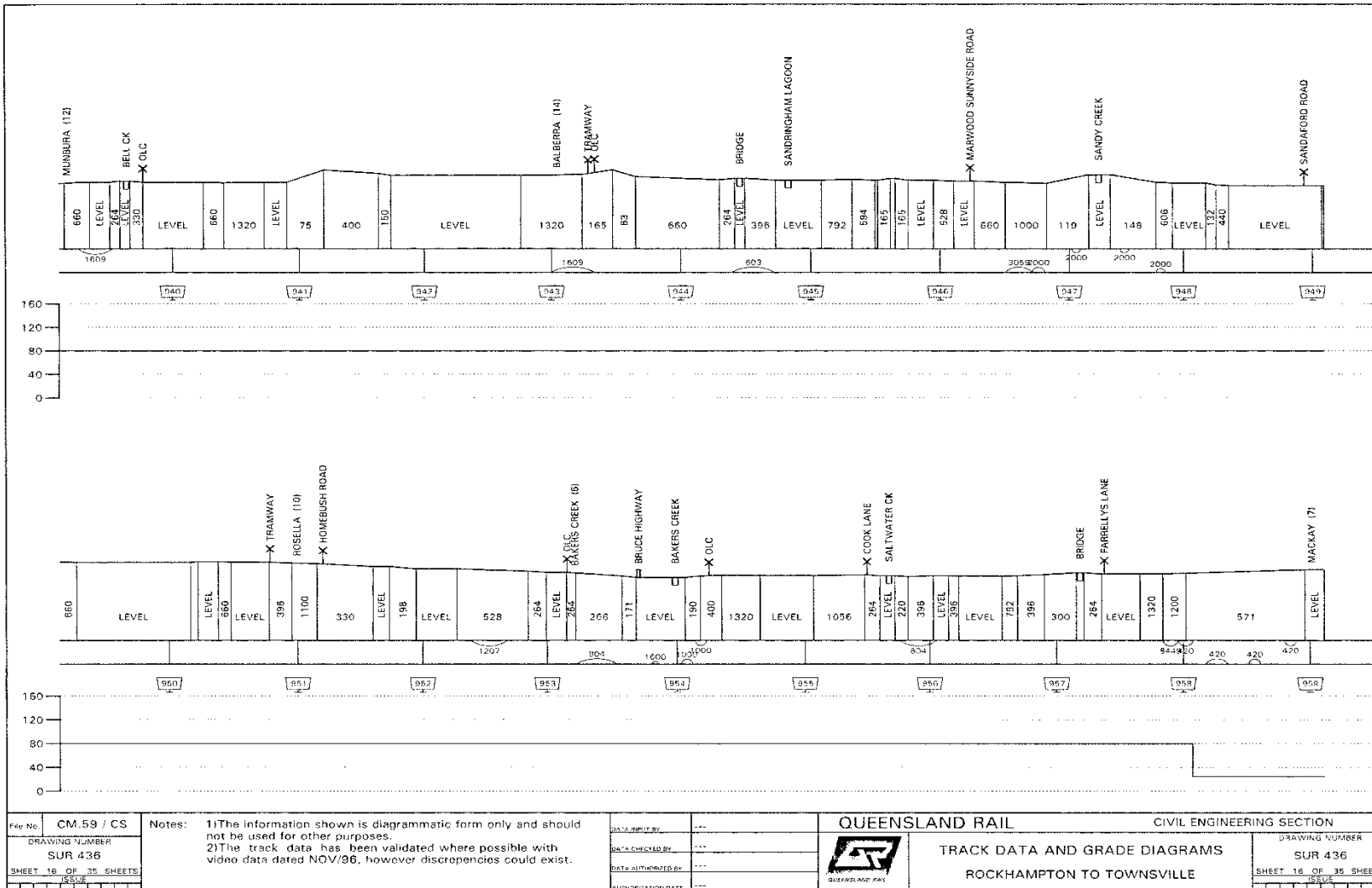
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2)The track data has been validated where possible with video data dated NOV:96, however discrepancies could exist.

DATE PREPARED BY: ---
DATE CHECKED BY: ---
DATE APPROVED BY: ---
DATE IN PROGRESS DATA: ---

QUEENSLAND RAIL CIVIL ENGINEERING SECTION
TRACK DATA AND GRADE DIAGRAMS
ROCKHAMPTON TO TOWNVILLE

DRAWING NUMBER: SUR 436
SHEET 15 OF 35 SHEETS


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 DRAWING NUMBER
 SUR 436
 SHEET 16 OF 35 SHEETS

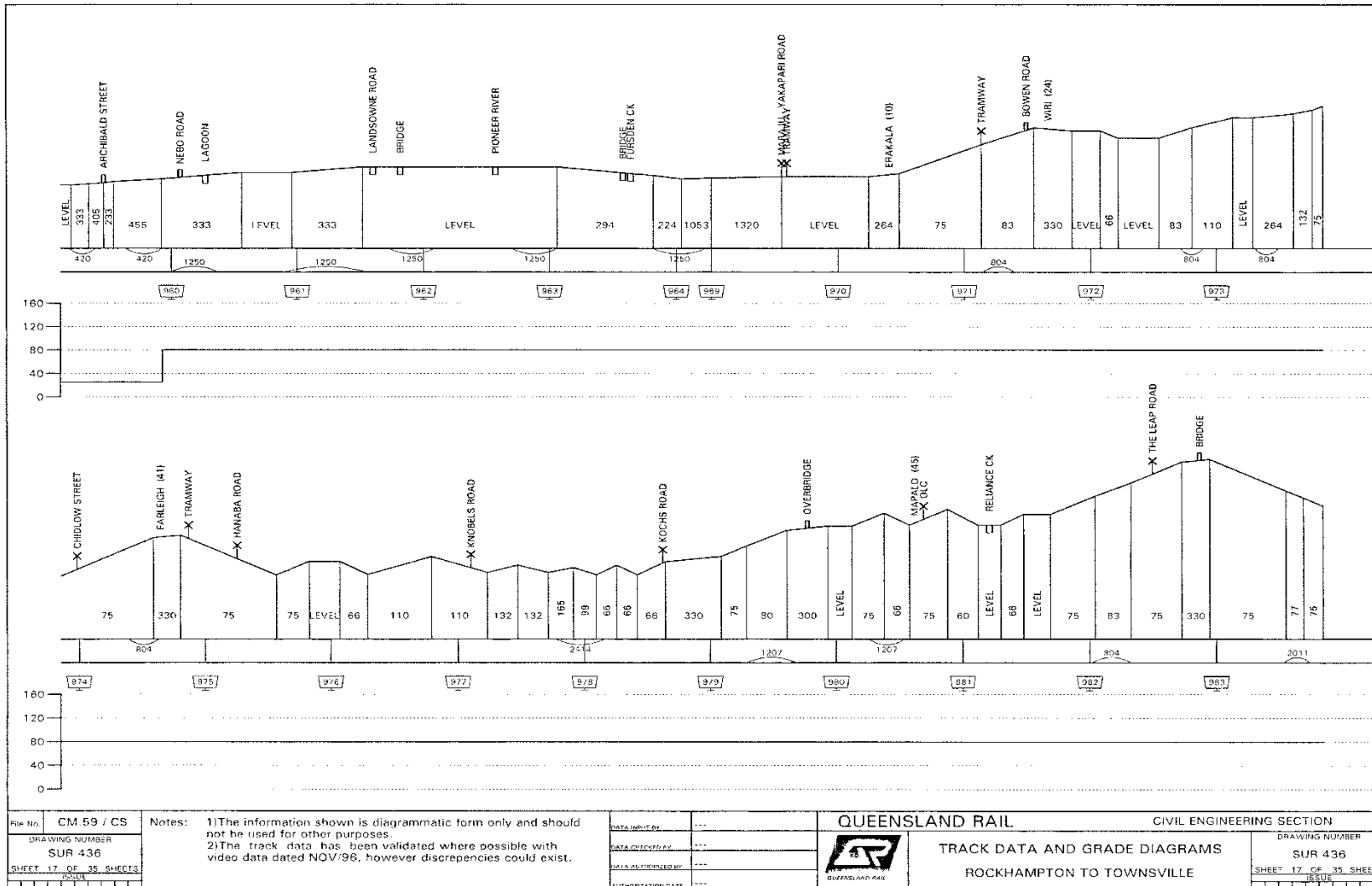
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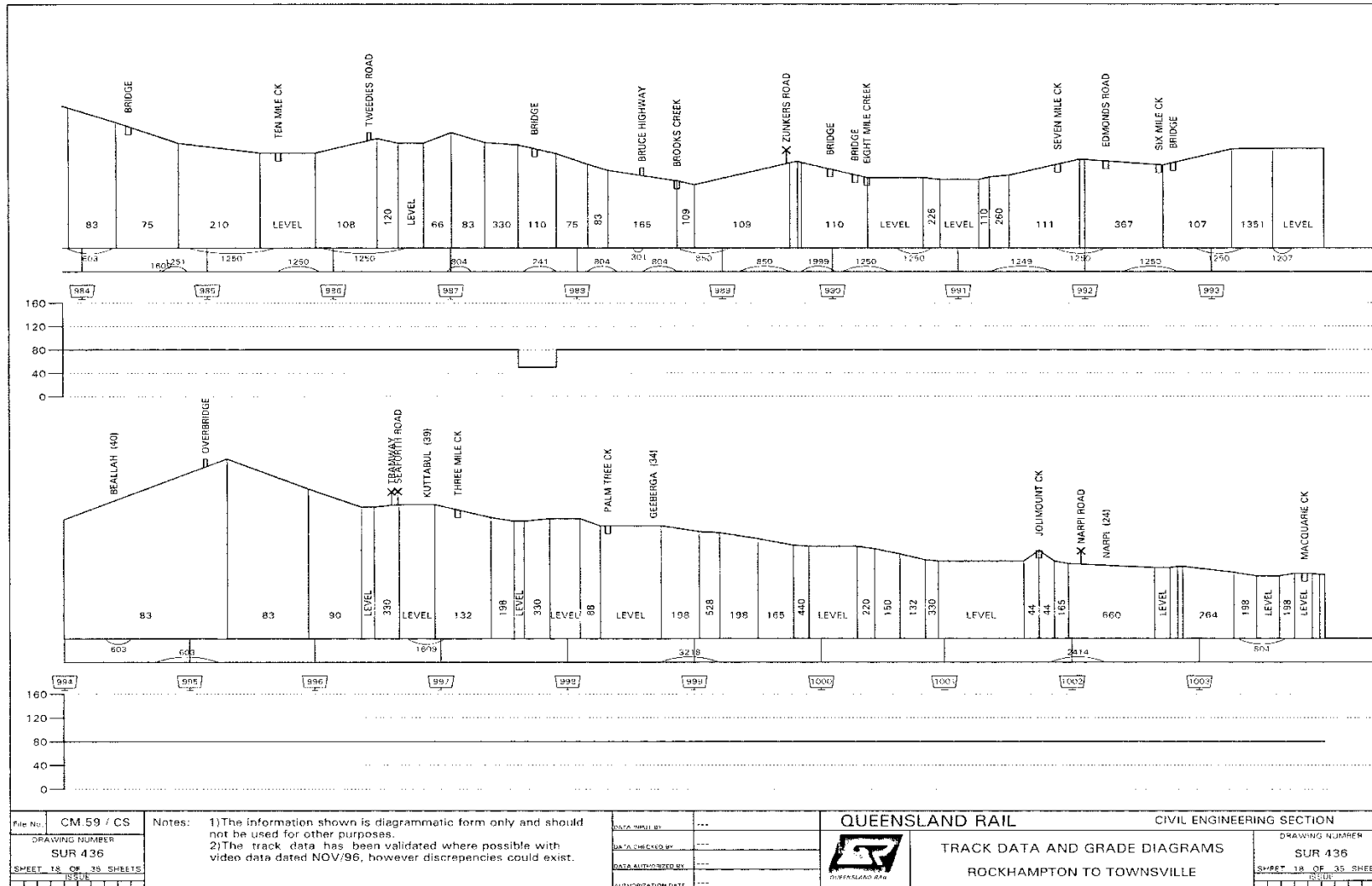
QUEENSLAND RAIL CIVIL ENGINEERING SECTION

 TRACK DATA AND GRADE DIAGRAMS
 ROCKHAMPTON TO TOWNVILLE

DRAWING NUMBER
 SUR 436
 SHEET 16 OF 35 SHEETS

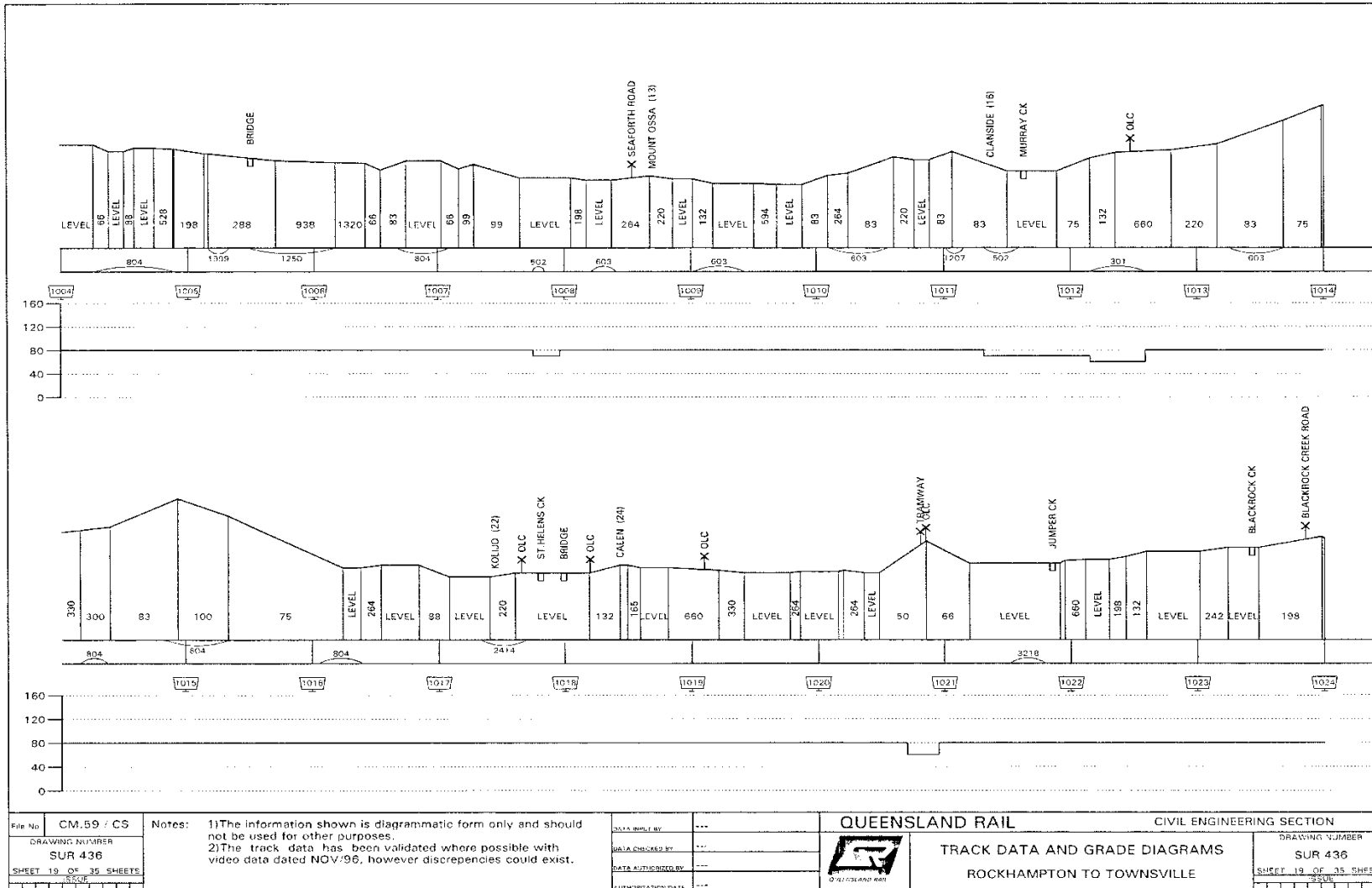
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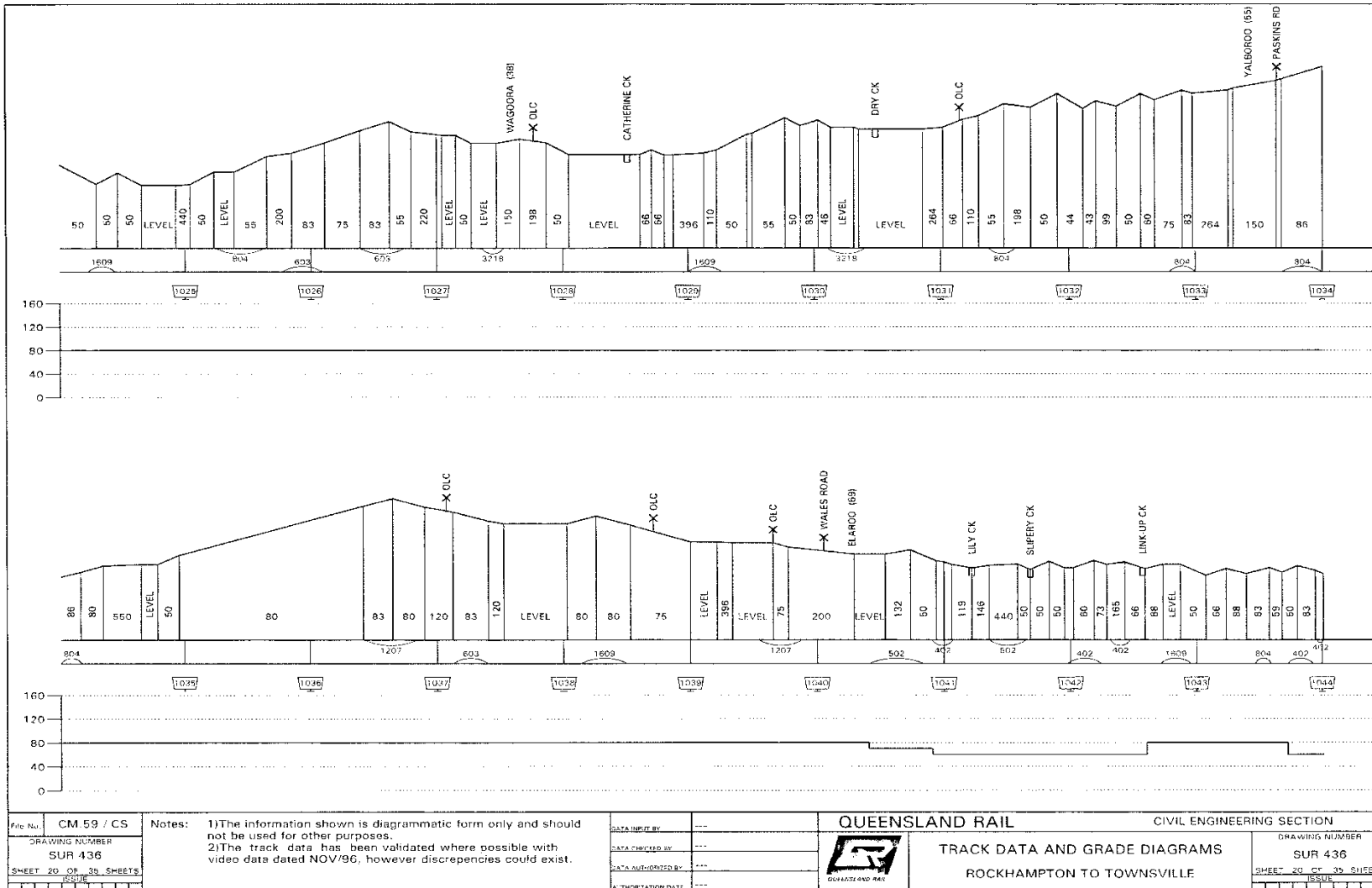


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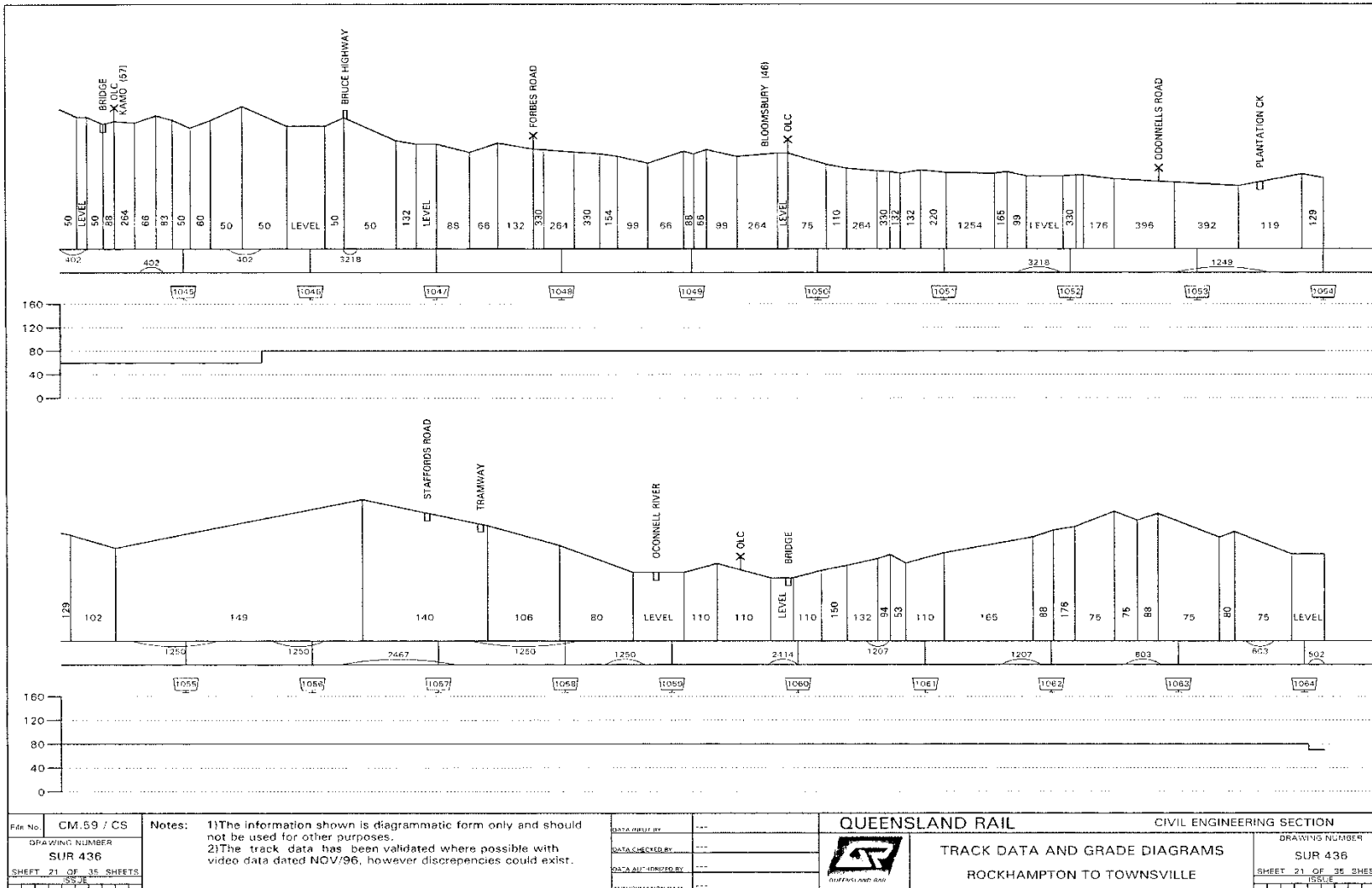



File No CM.59 / CS	Notes: 1)The information shown is diagrammatic form only and should not be used for other purposes. 2)The track data has been validated where possible with video data dated NOV/96, however discrepancies could exist.	DESIGNED BY ---	QUEENSLAND RAIL	CIVIL ENGINEERING SECTION
DRAWING NUMBER SUR 436		CHECKED BY ---		DRAWING NUMBER SUR 436
SHEET 19 OF 35 SHEETS		DATA AUTHORIZED BY ---	TRACK DATA AND GRADE DIAGRAMS	SHEET 19 OF 35 SHEETS
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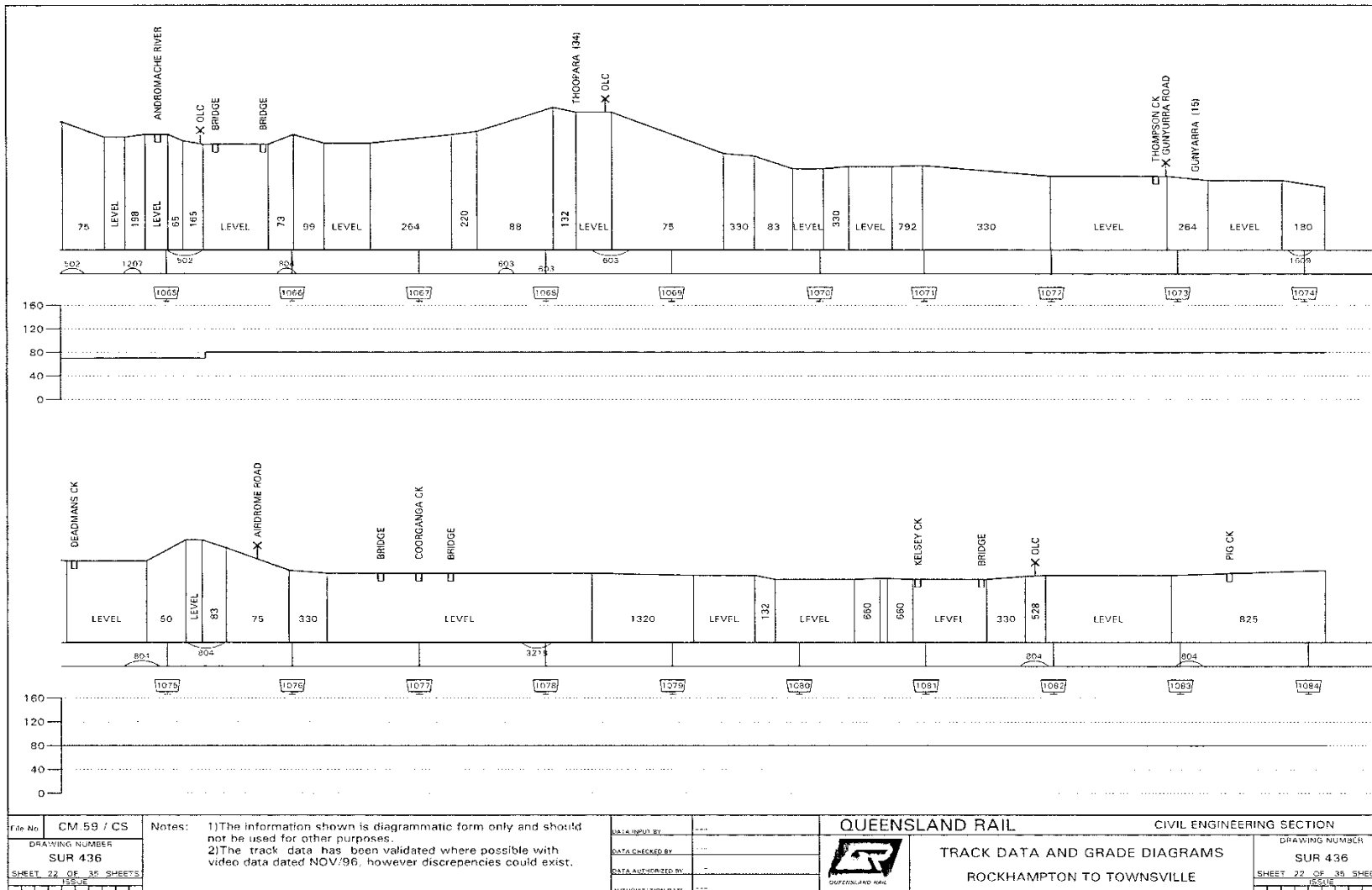


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FAR No. CM.59 / CS DRAWING NUMBER SUR 436 SHEET 21 OF 35 SHEETS ISSUE	Notes: 1)The information shown is diagrammatic form only and should not be used for other purposes. 2)The track data has been validated where possible with video data dated NOV/96, however discrepancies could exist.	DATA CHECKED BY: --- DATA CHECKED BY: --- DATA APPROVED BY: --- DATA APPROVED BY: ---	QUEENSLAND RAIL  CIVIL ENGINEERING SECTION	TRACK DATA AND GRADE DIAGRAMS ROCKHAMPTON TO TOWNVILLE	DRAWING NUMBER SUR 436 SHEET 21 OF 35 SHEETS ISSUE
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File No: CM.59 / CS
 DRAWING NUMBER: SUR 436
 SHEET 22 OF 35 SHEETS

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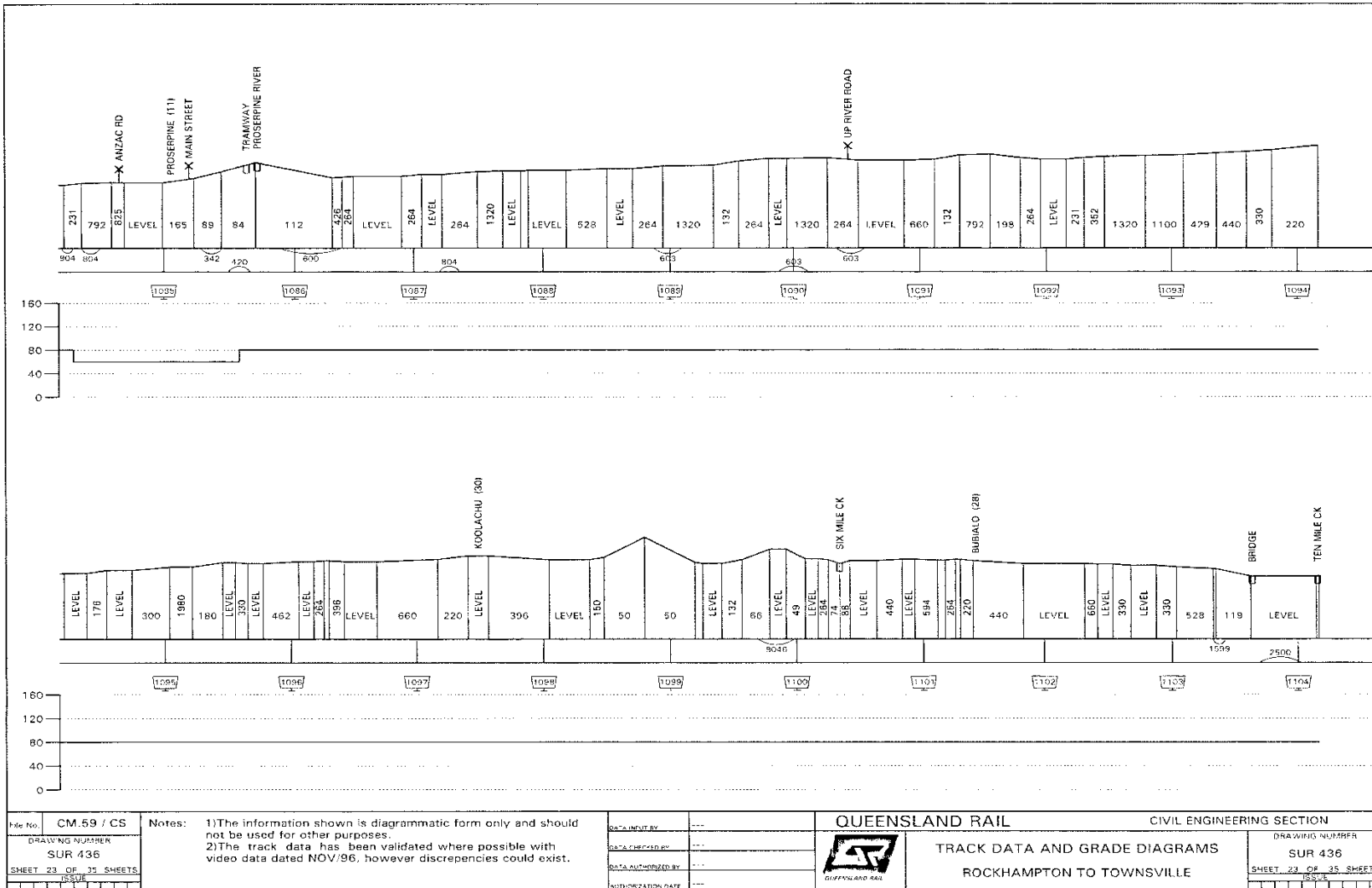
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QUEENSLAND RAIL
 TRACK DATA AND GRADE DIAGRAMS
 ROCKHAMPTON TO TOWNVILLE

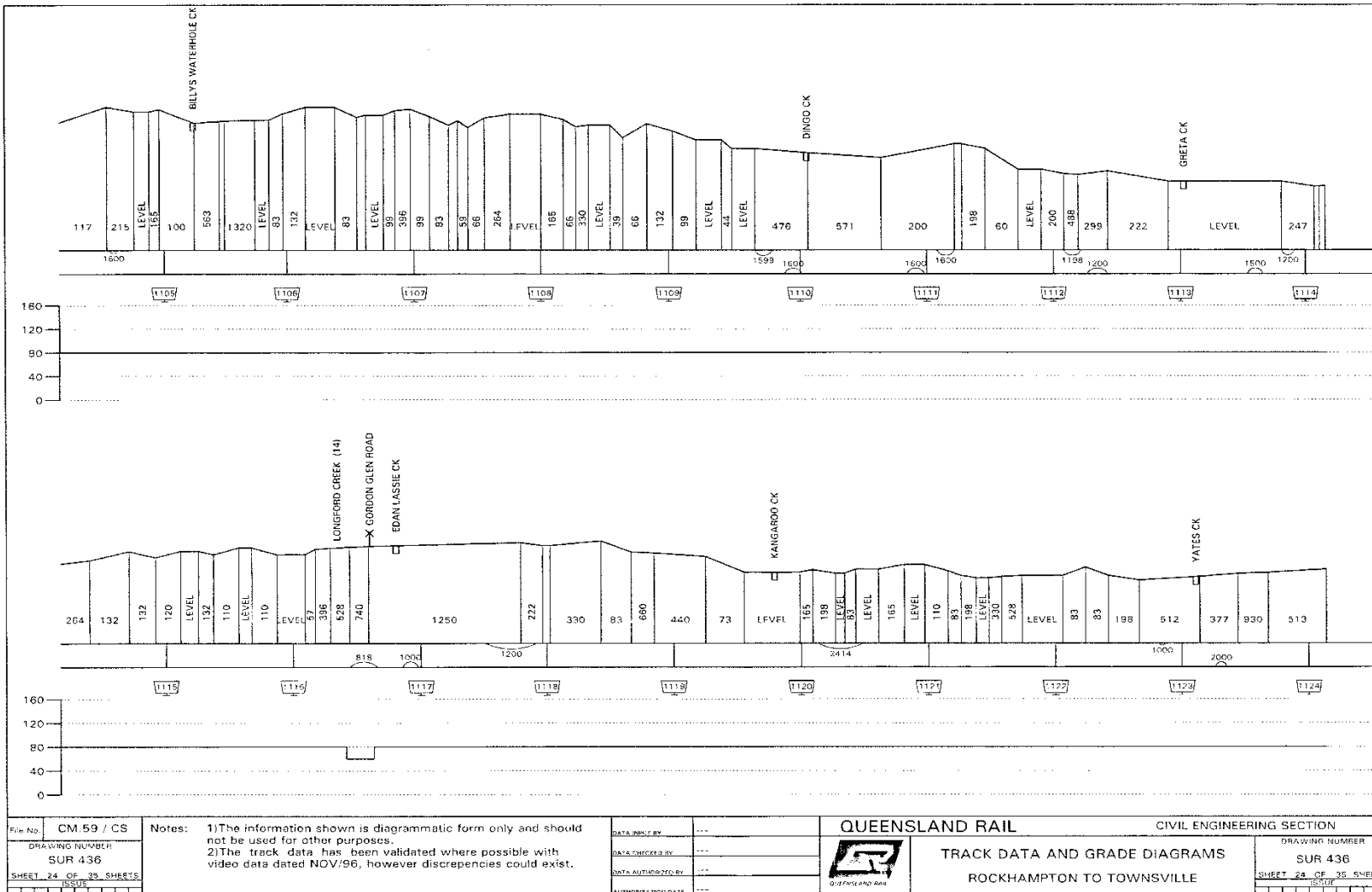
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 SHEET 22 OF 35 SHEETS


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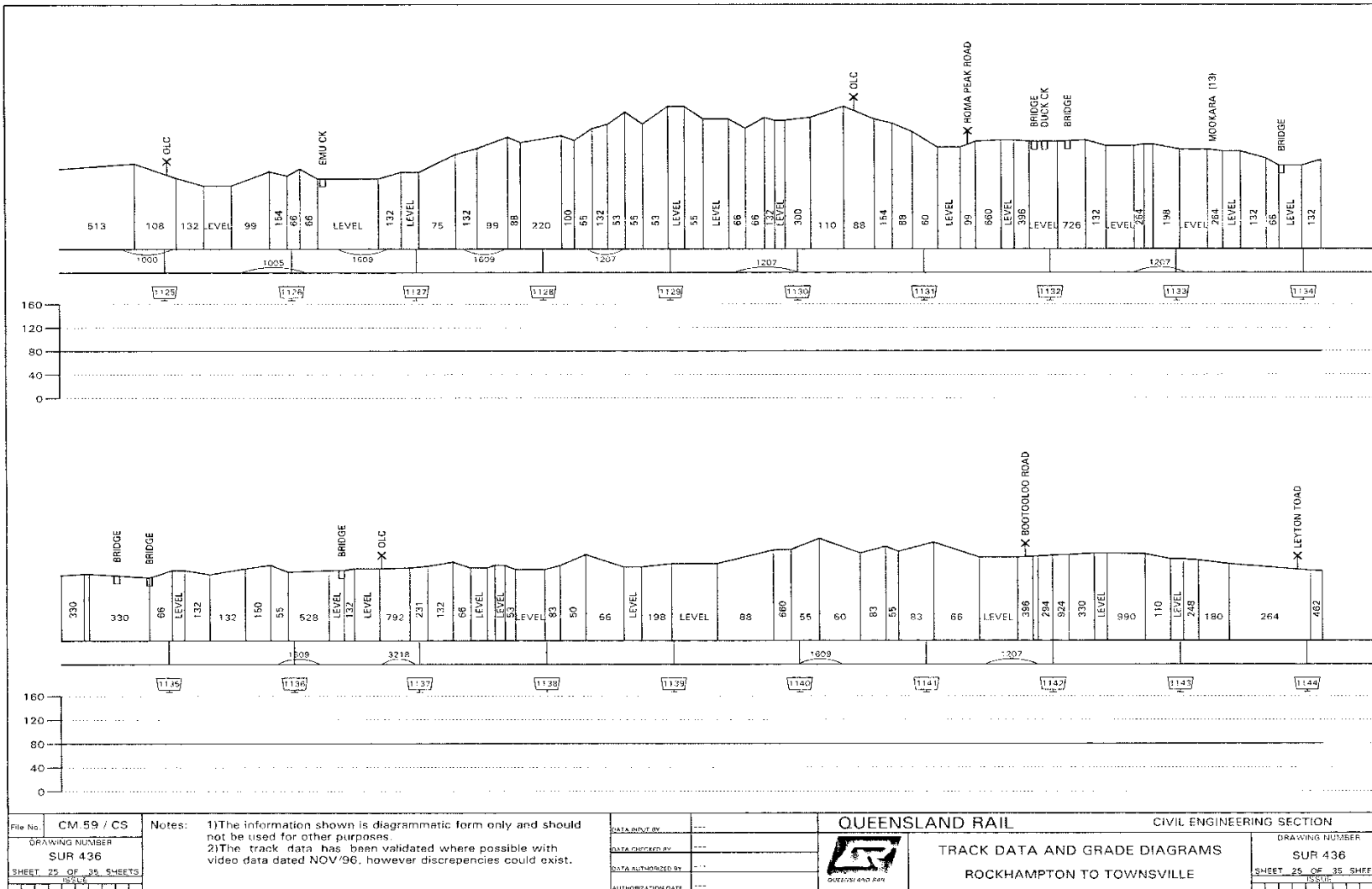
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DRAWING NUMBER SUR 436		DATA CHECKED BY	---		TRACK DATA AND GRADE DIAGRAMS	SHEET 23 OF 35 SHEETS
SHEET 23 OF 35 SHEETS		DATA AUTHORIZED BY	---		ROCKHAMPTON TO TOWNVILLE	DATE
DATE	---	AUTHORIZATION DATE	---			


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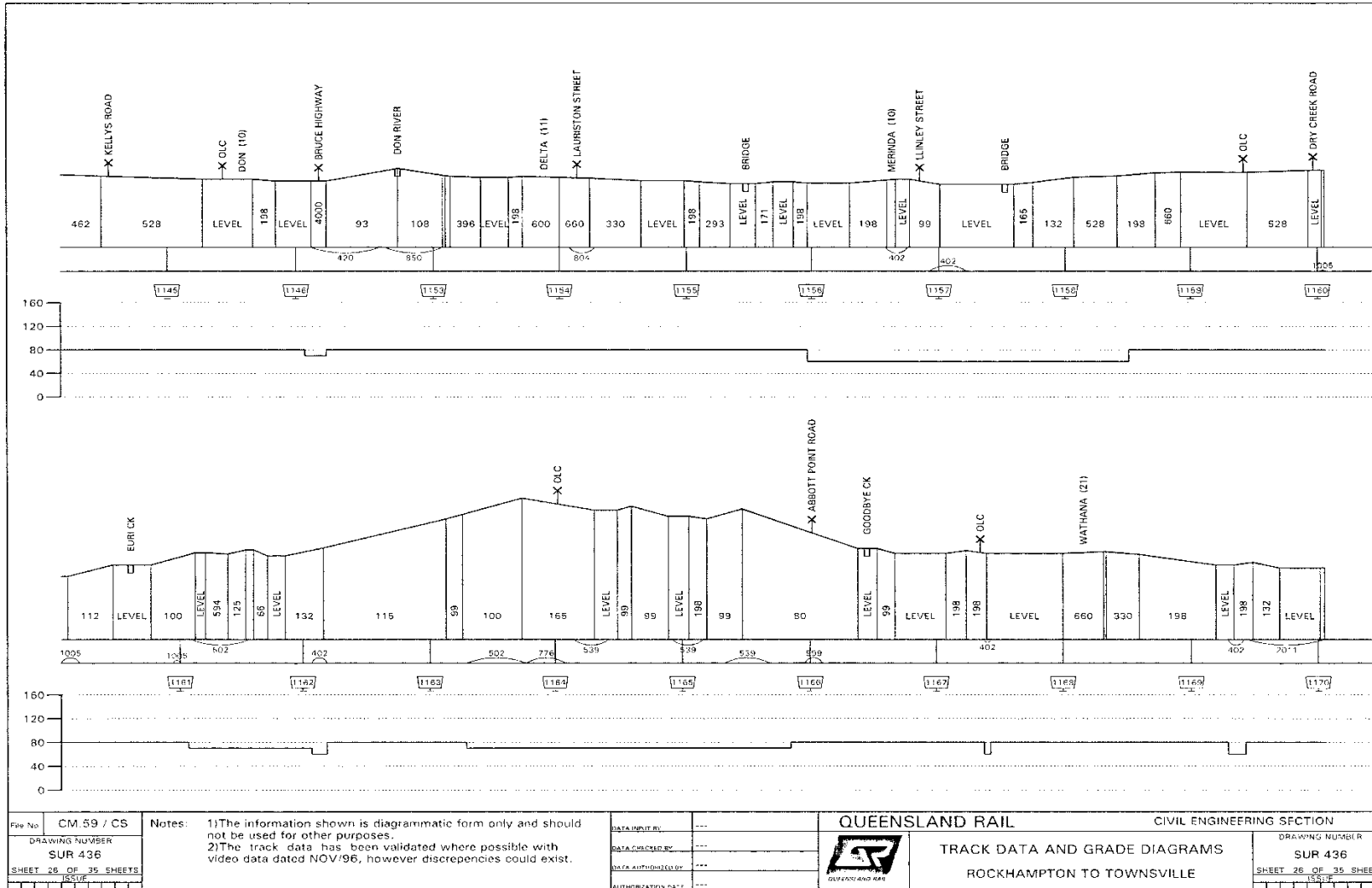
File No: CM.59 / CS Drawing Number: SUR 436 Sheet 24 of 35 Sheets	Notes: 1) The information shown is diagrammatic form only and should not be used for other purposes. 2) The track data has been validated where possible with video data dated NOV/96, however discrepancies could exist.	Data Input By: --- Data Checked By: --- Data Authenticated By: --- Information Date: ---	QUEENSLAND RAIL  TRACK DATA AND GRADE DIAGRAMS ROCKHAMPTON TO TOWNSVILLE	CIVIL ENGINEERING SECTION Drawing Number: SUR 436 Sheet 24 of 35 Sheets
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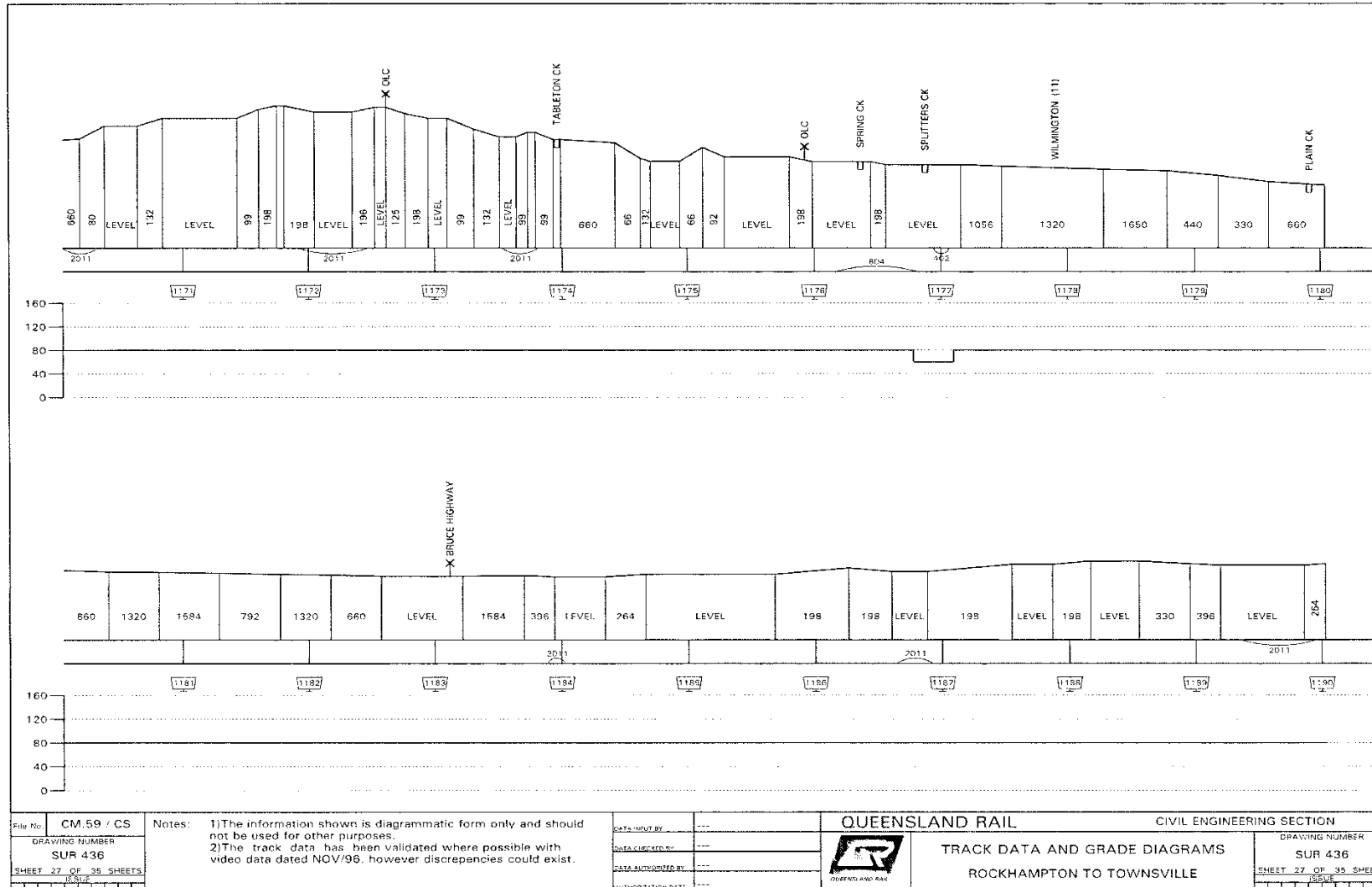


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SHEET 25 OF 35 SHEETS		DATA AUTHORIZED BY: ---		ROCKHAMPTON TO TOWNVILLE	
		AUTHORISATION DATE: ---			SHEET 25 OF 35 SHEETS

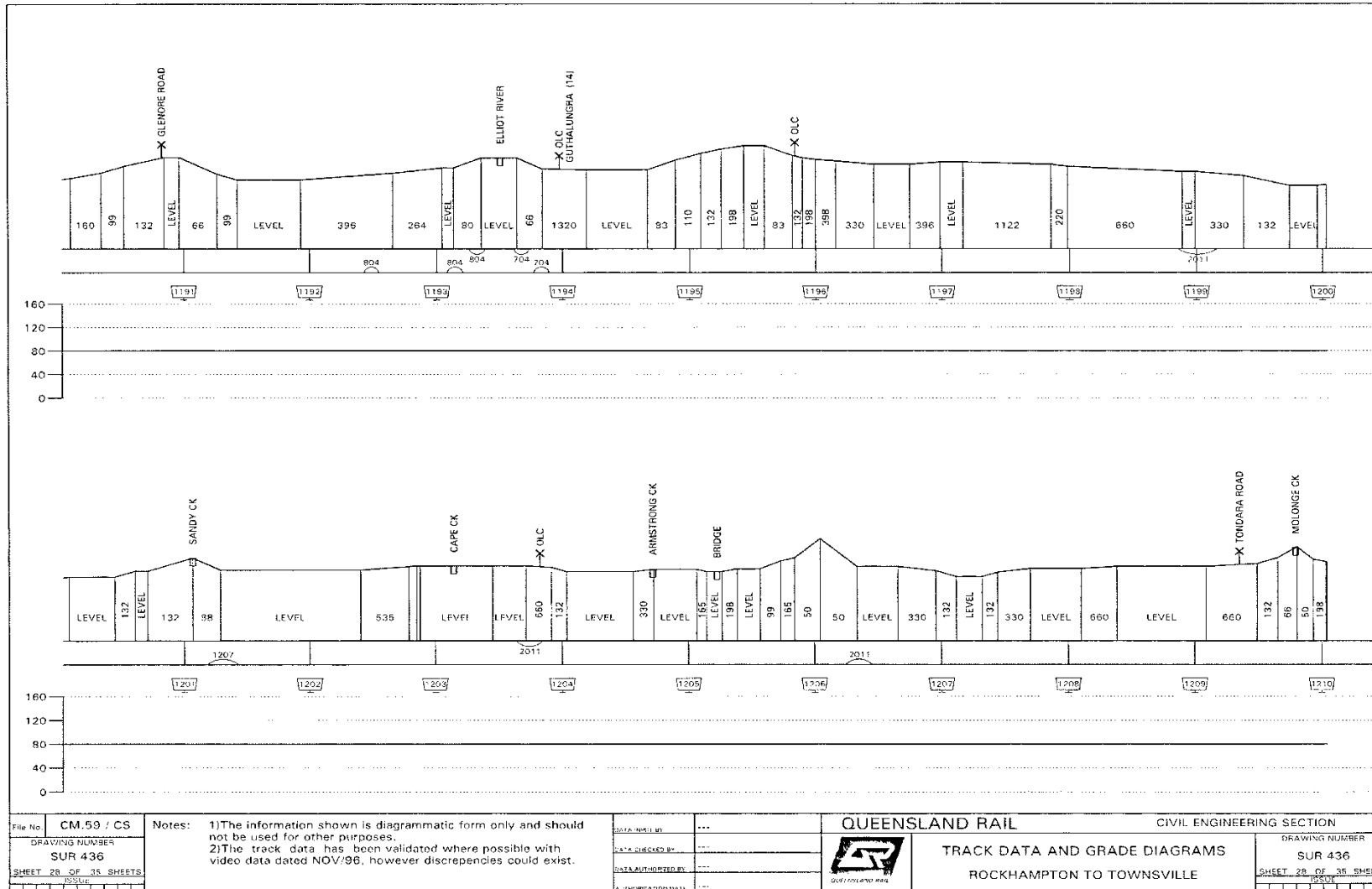
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File No: CM.59 / CS
 DRAWING NUMBER: SUR 436
 SHEET 28 OF 38 SHEETS

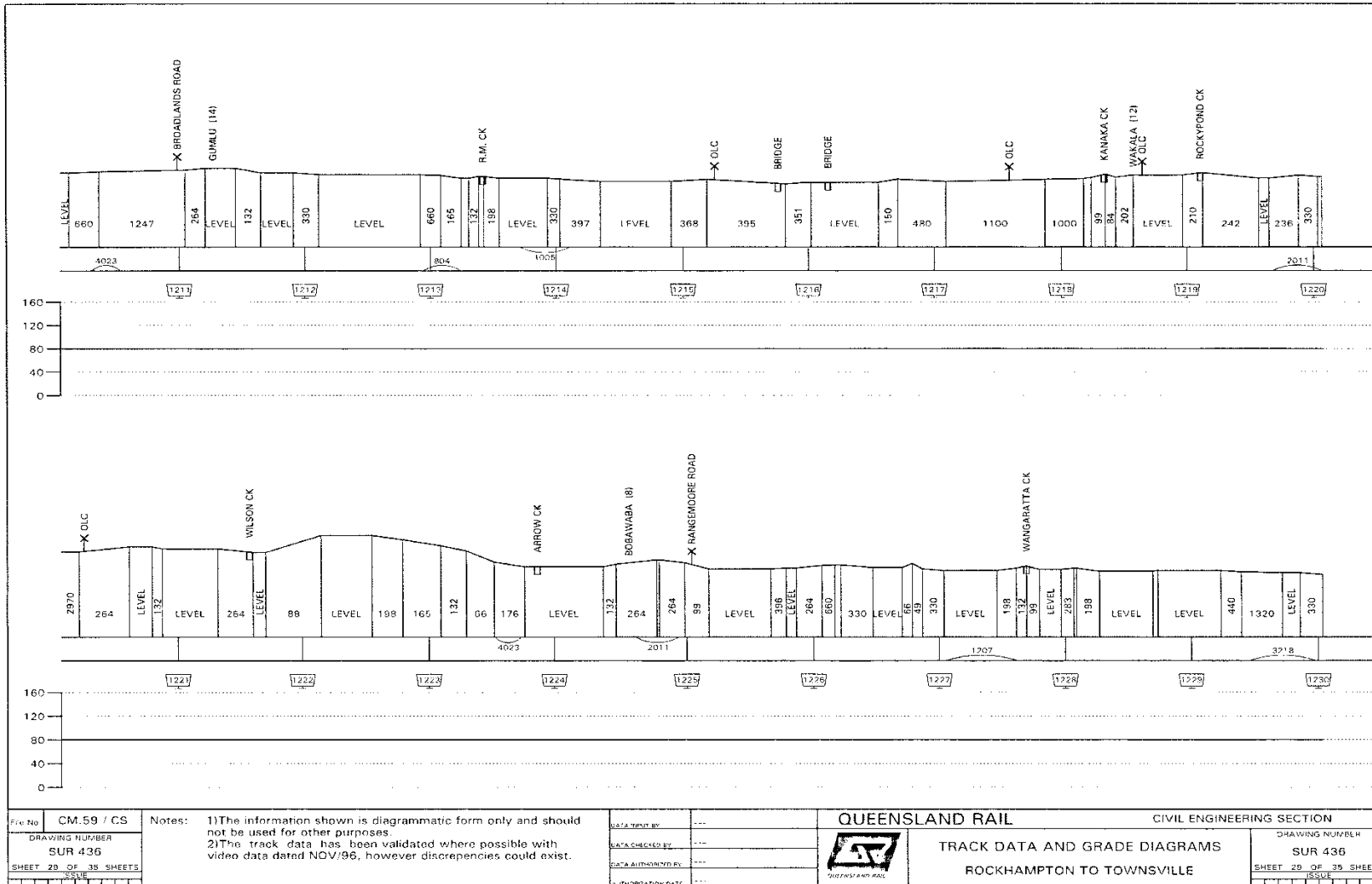
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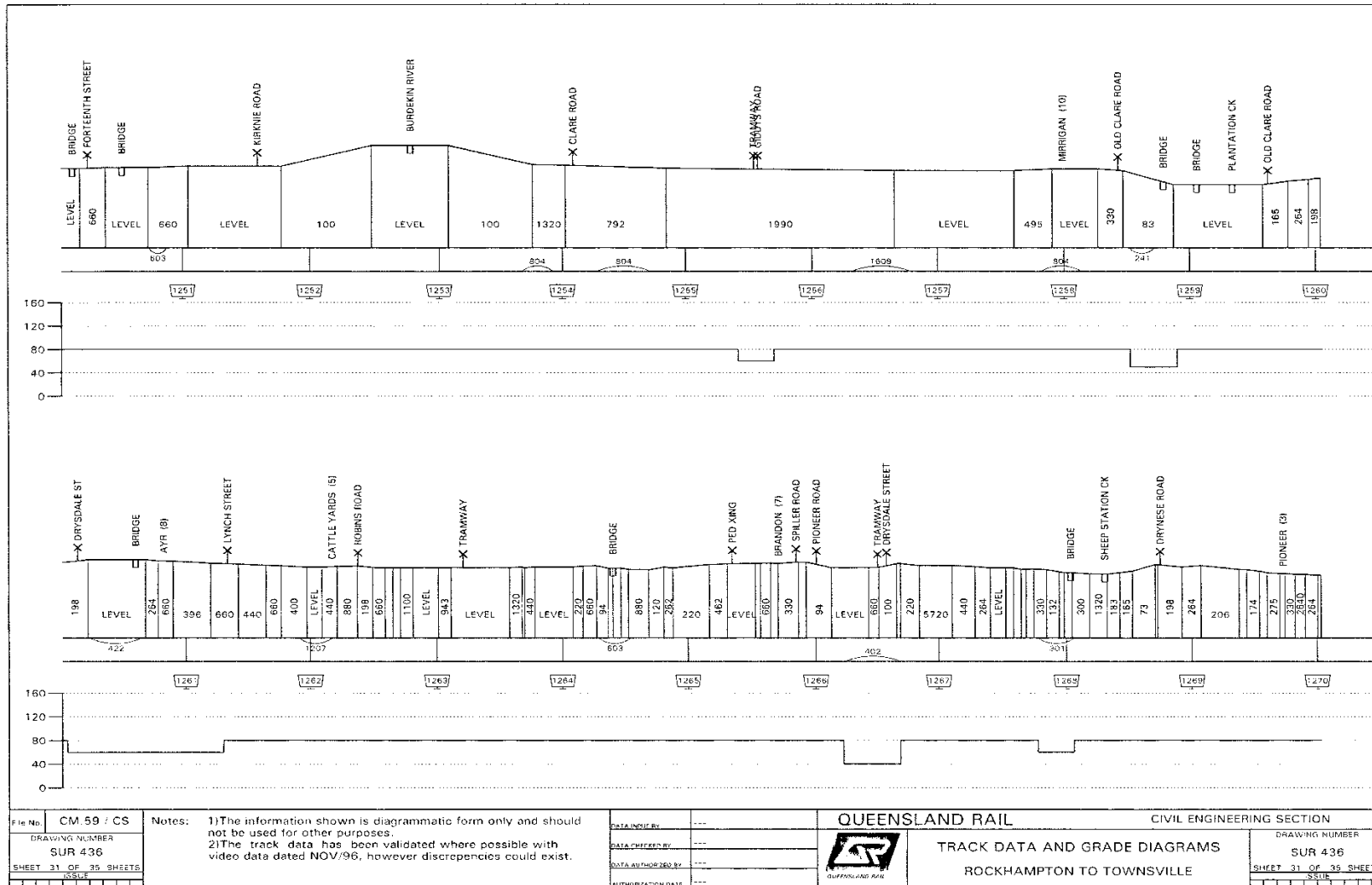
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 TRACK DATA AND GRADE DIAGRAMS
 ROCKHAMPTON TO TOWNVILLE

DRAWING NUMBER: SUR 436
 SHEET 28 OF 38 SHEETS

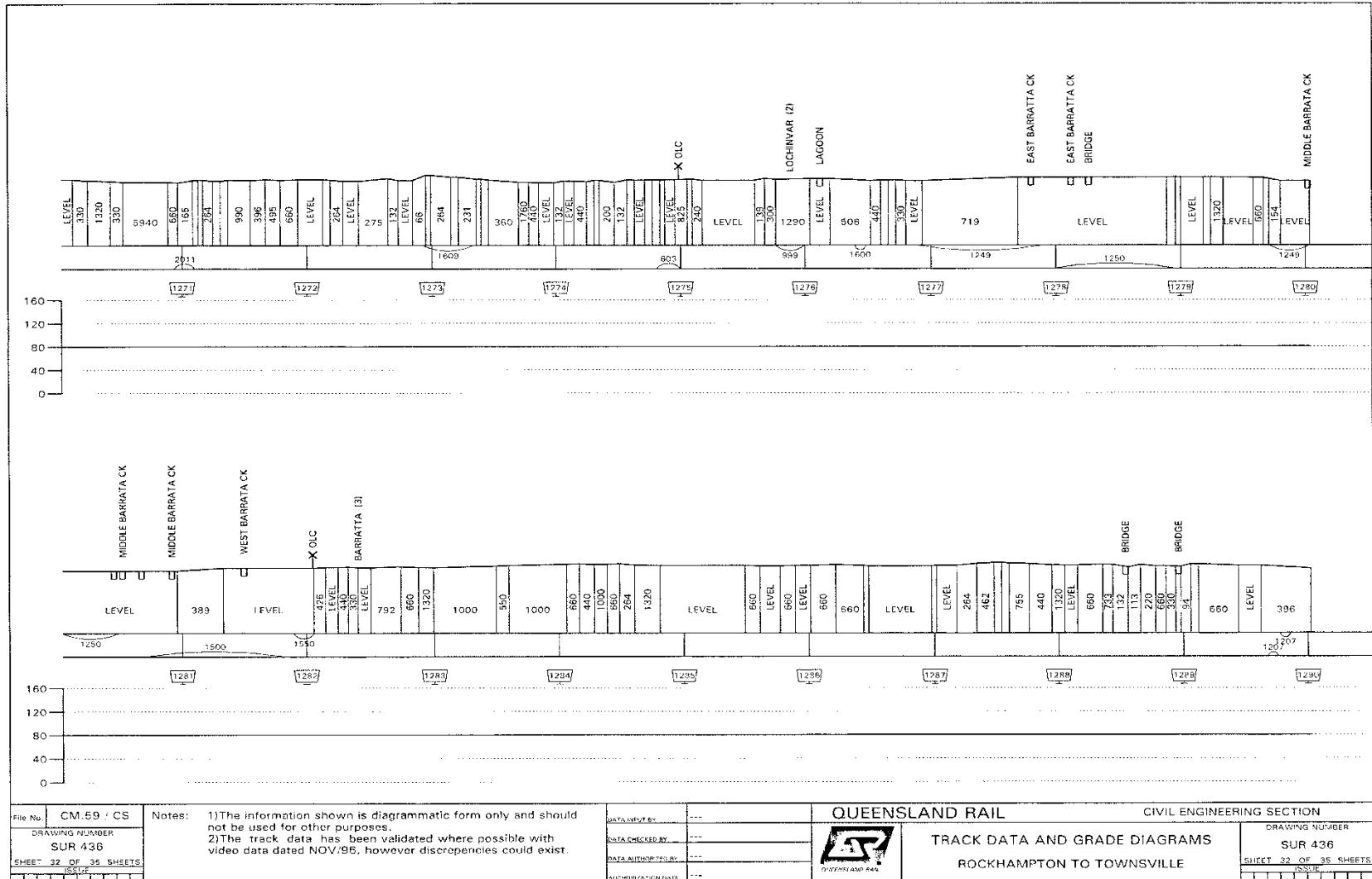
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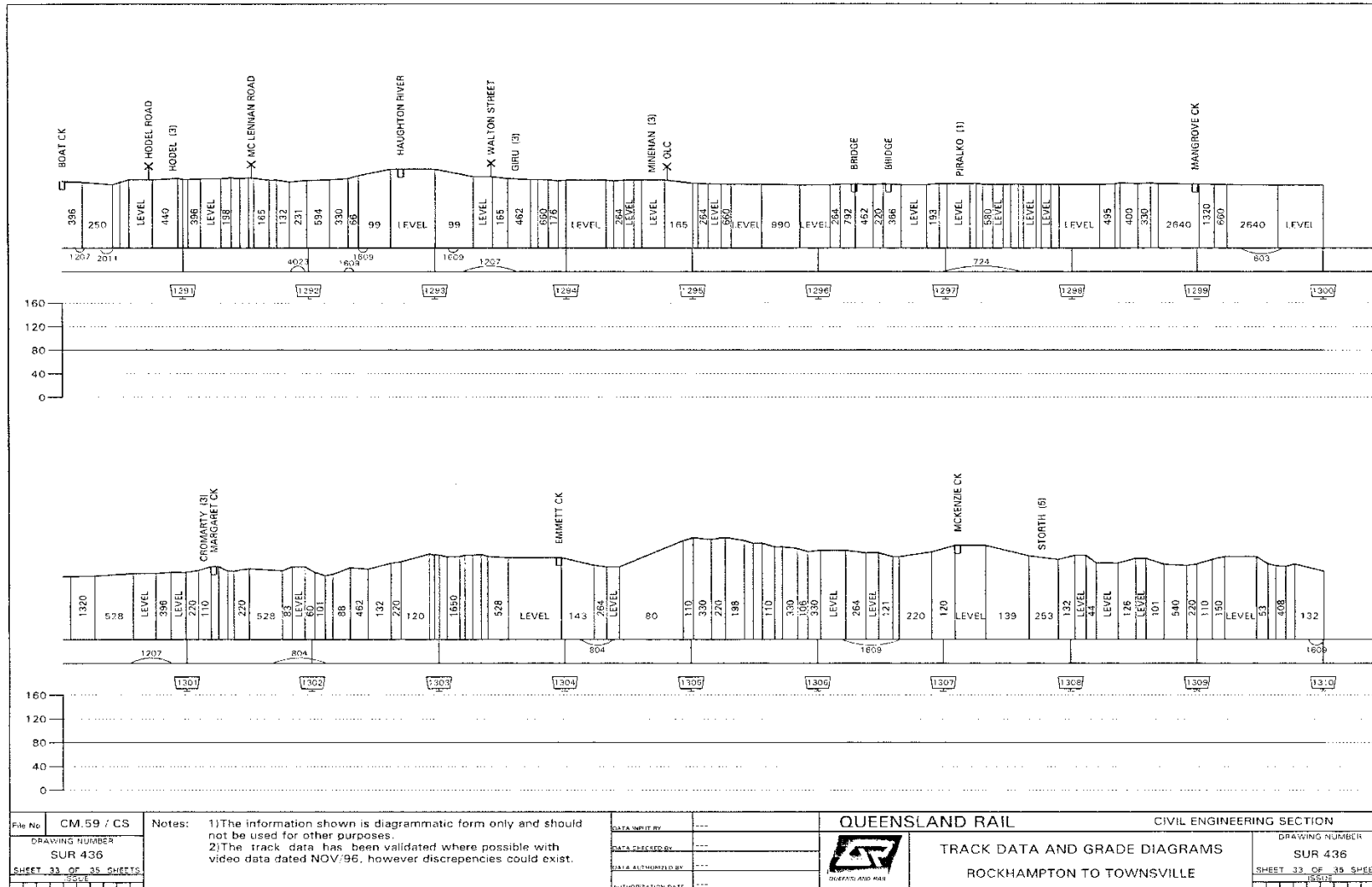
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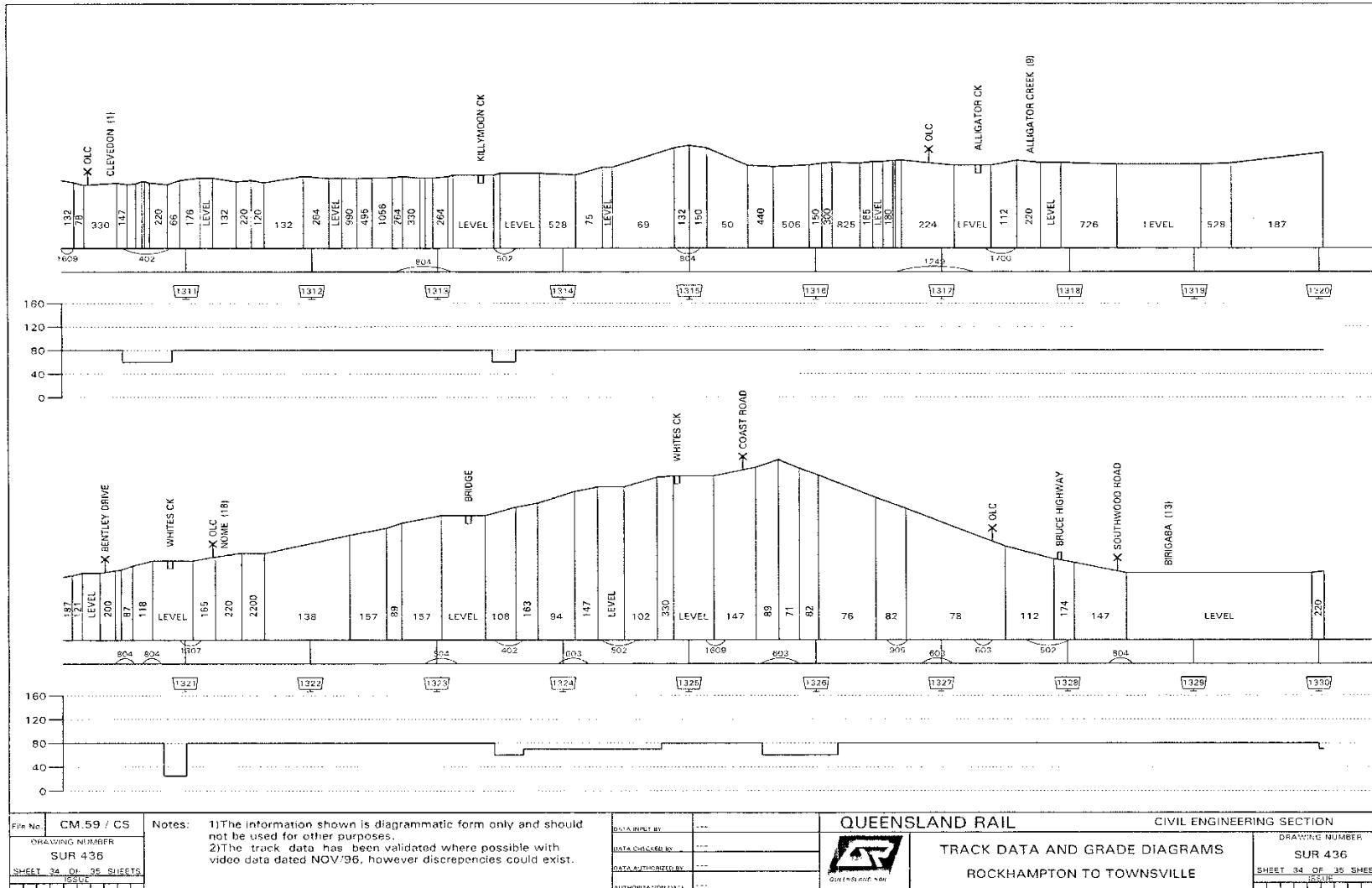


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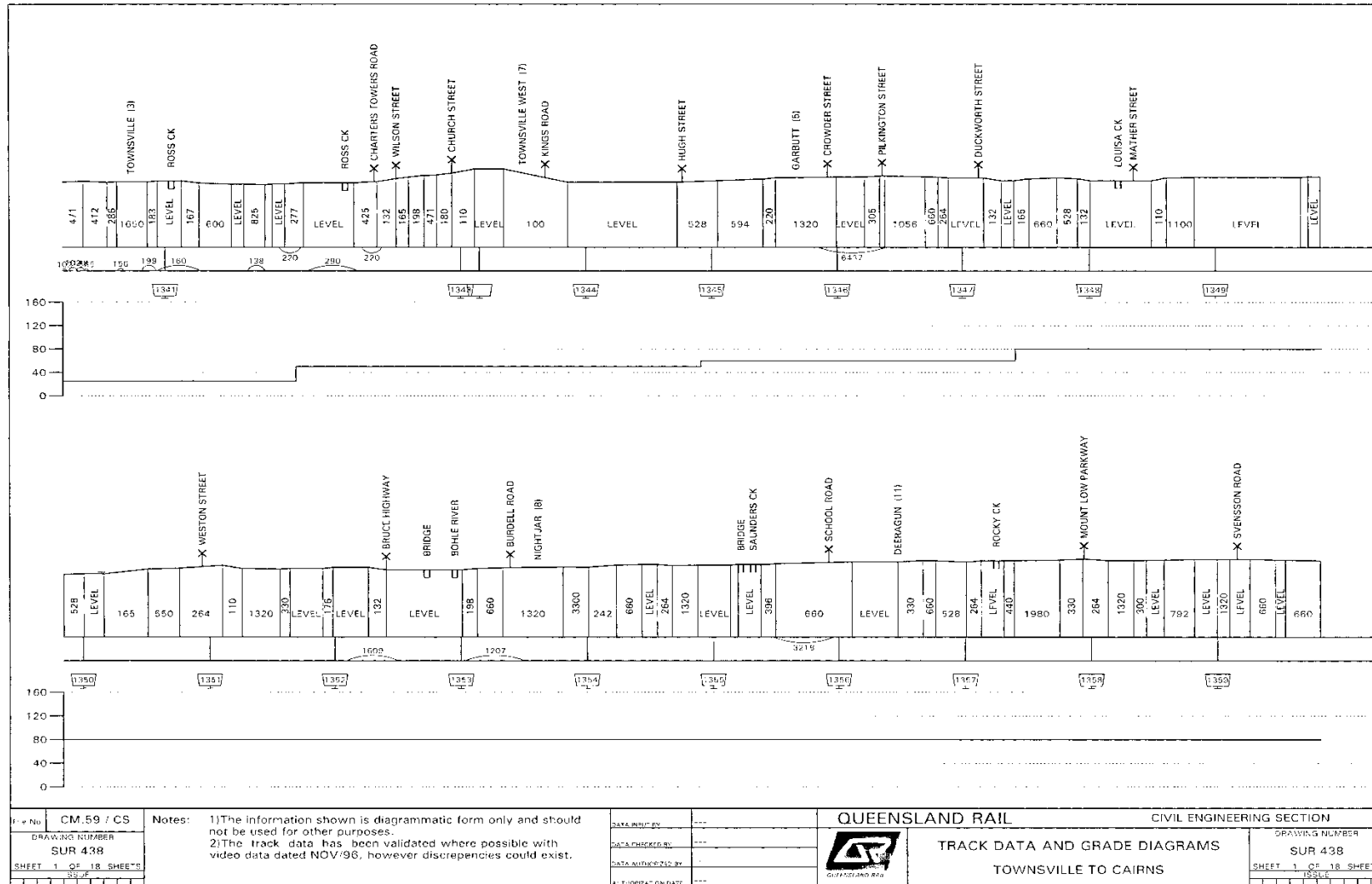


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SHEET 33 OF 35 SHEETS		DATA AUTHORIZED BY: ---	TRACK DATA AND GRADE DIAGRAMS	SHEET 33 OF 35 SHEETS
		AUTHORIZATION DATE: ---	ROCKHAMPTON TO TOWNVILLE	

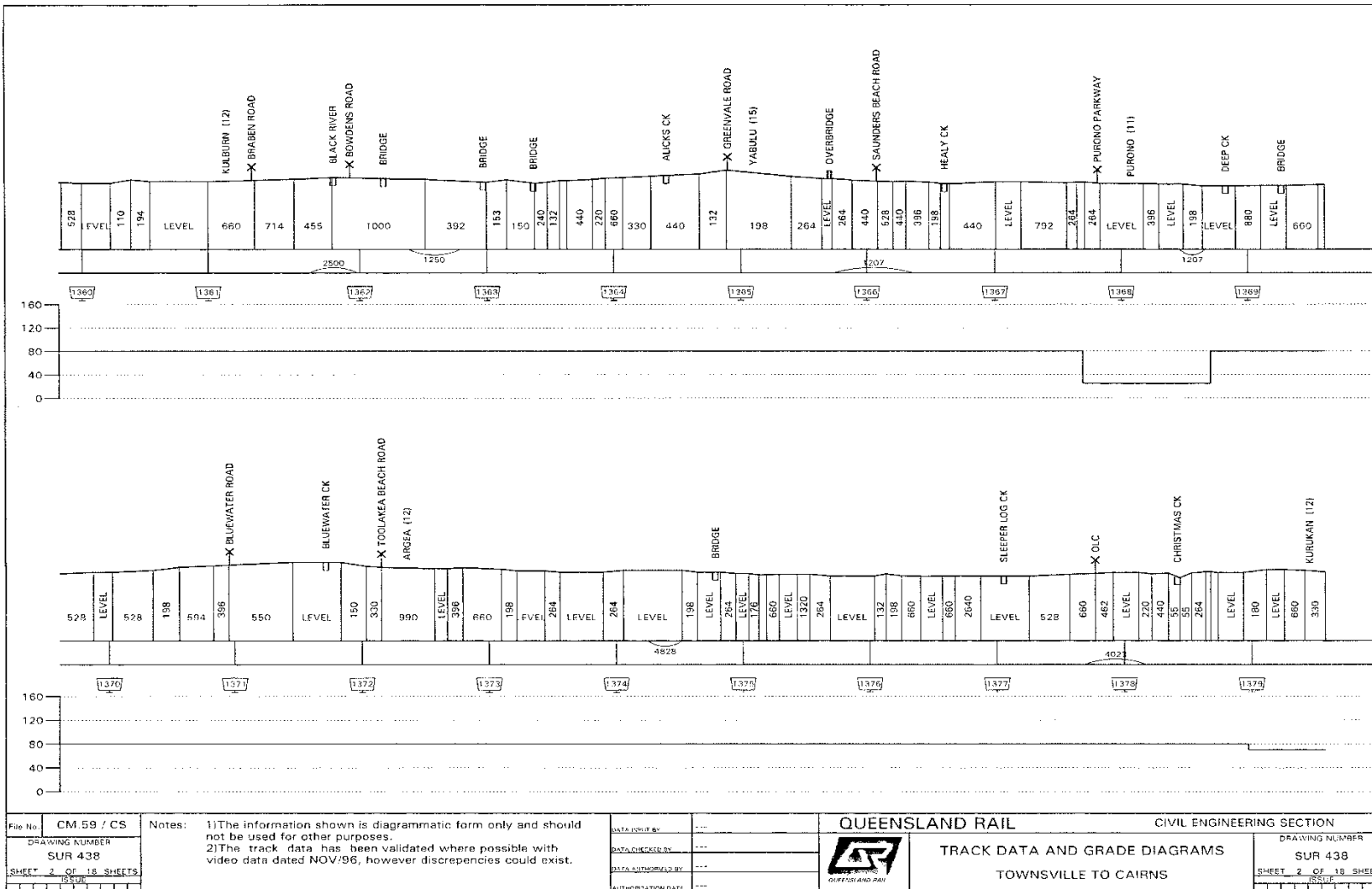
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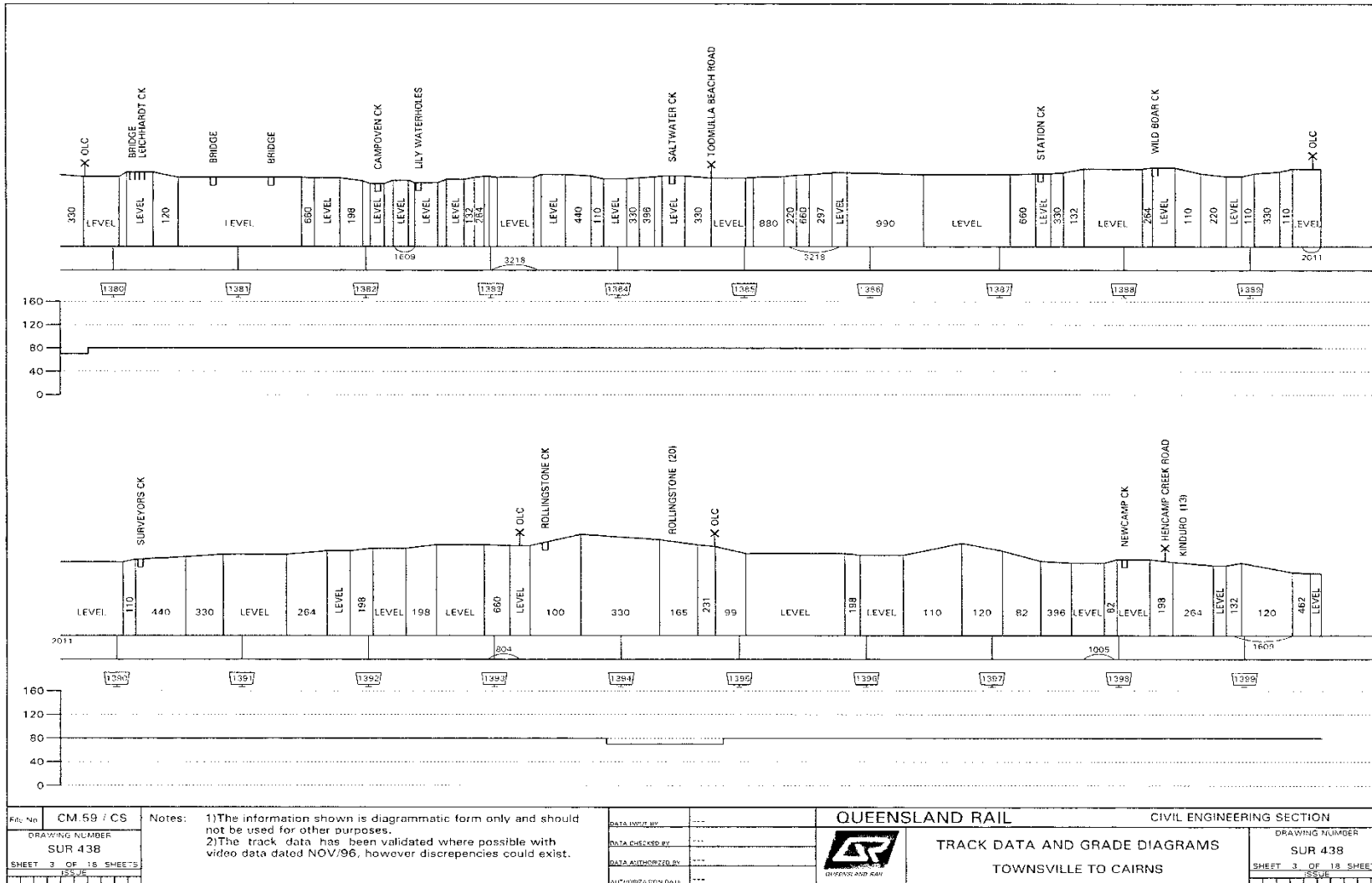


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SHEET 2 OF 18 SHEETS		AUTOCORRELATION DATE	---		AUTOCORRELATION DATE	---	
				DRAWING NUMBER SUR 438		SHEET 2 OF 18 SHEETS	

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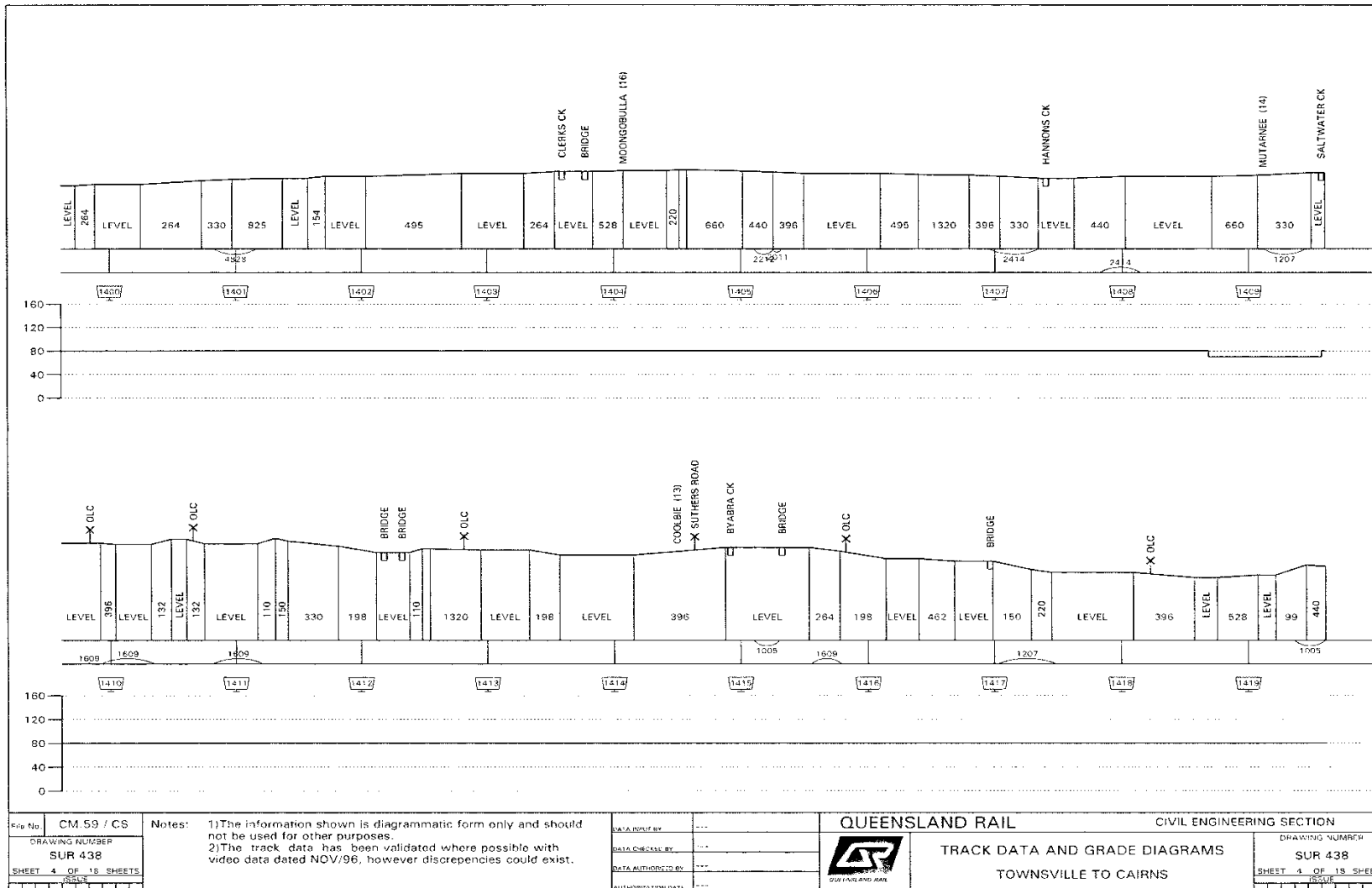



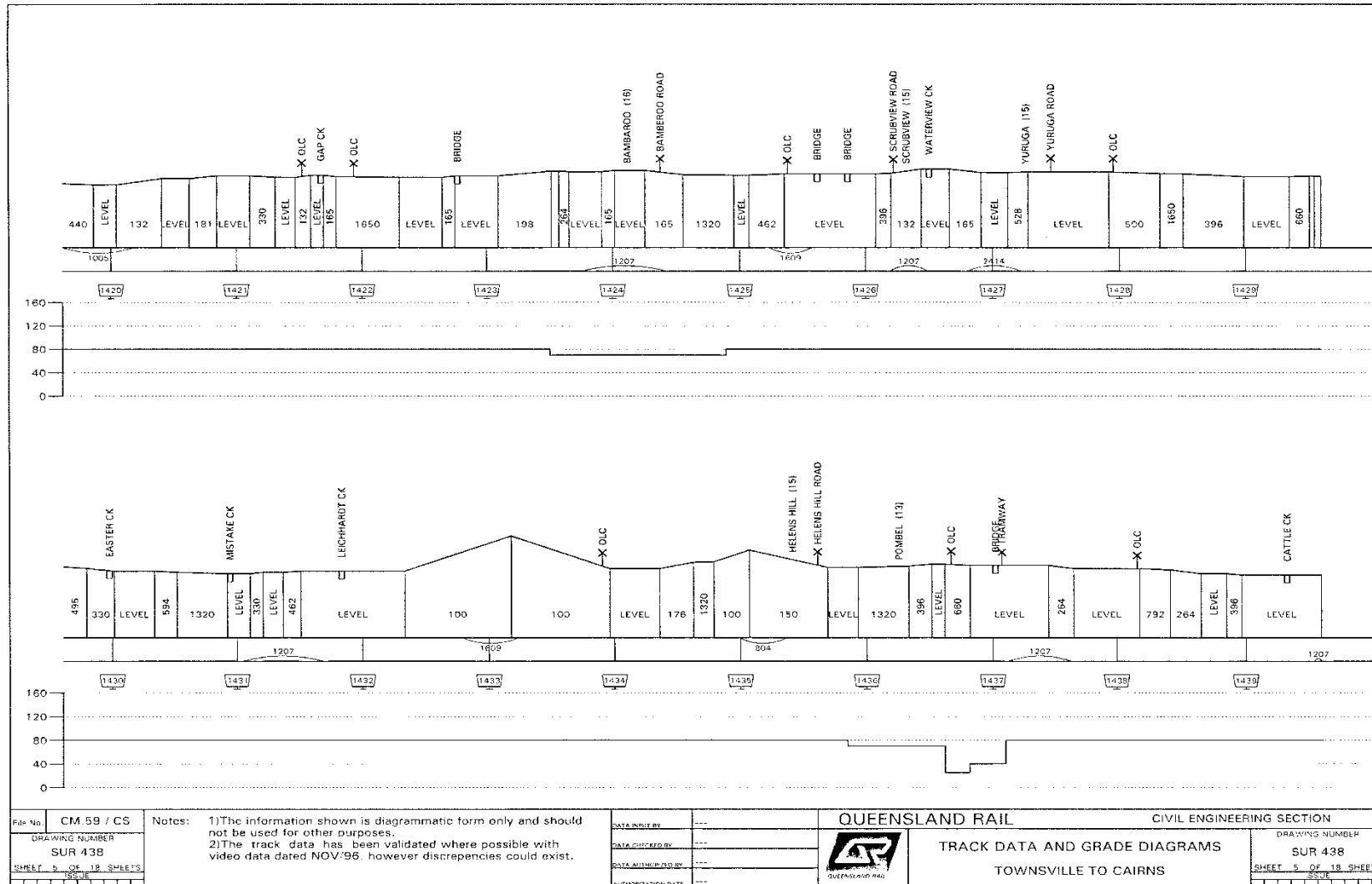
Fig No: CM 59 / CS
 DRAWING NUMBER: SUR 438
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Notes: 1)The information shown is diagrammatic form only and should not be used for other purposes.
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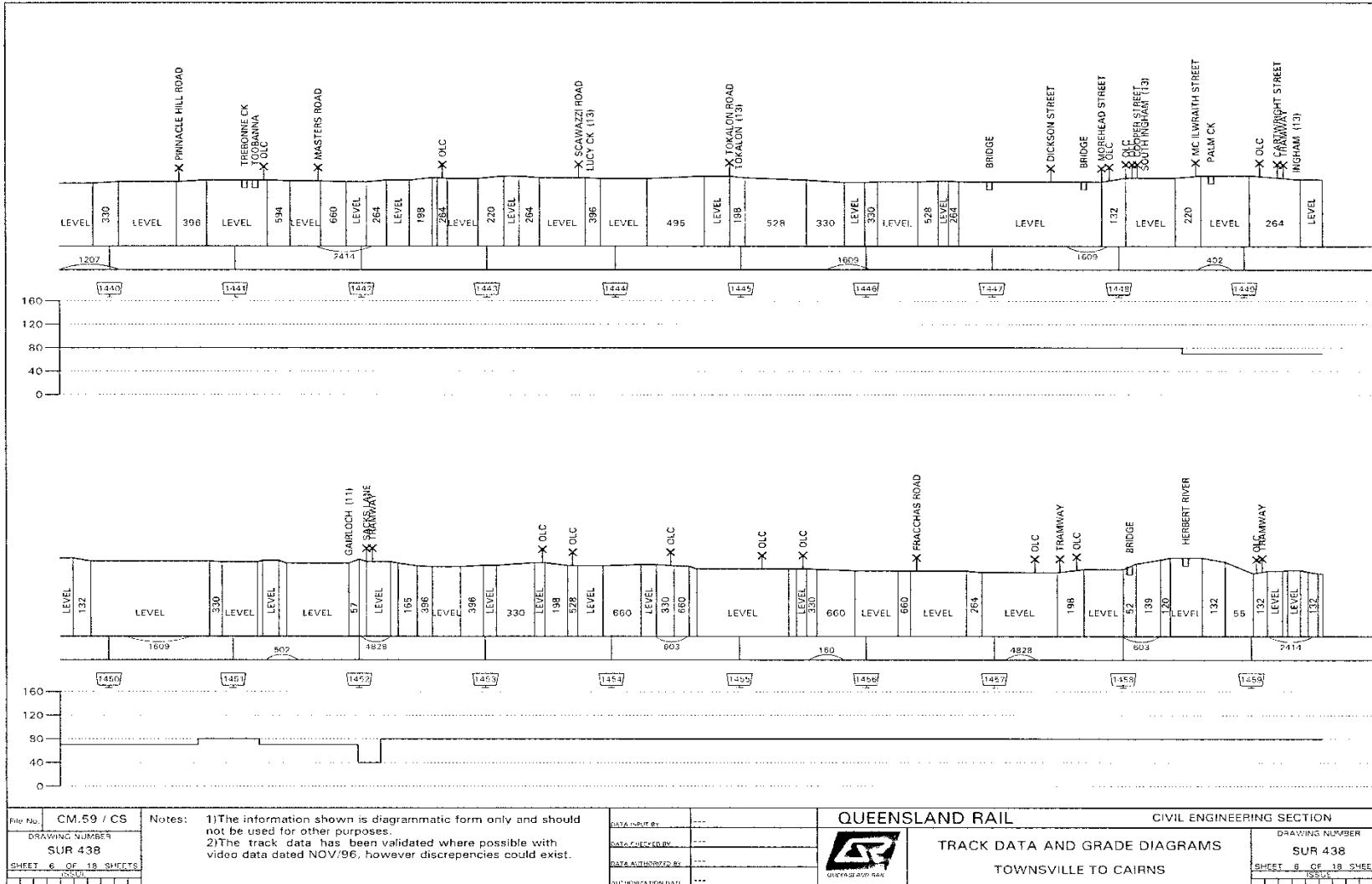
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 DATA CHECKED BY: ---
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QUEENSLAND RAIL		CIVIL ENGINEERING SECTION
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TRACK DATA AND GRADE DIAGRAMS		SHEET 4 OF 18 SHEETS
TOWNVILLE TO CAIRNS		REVISED

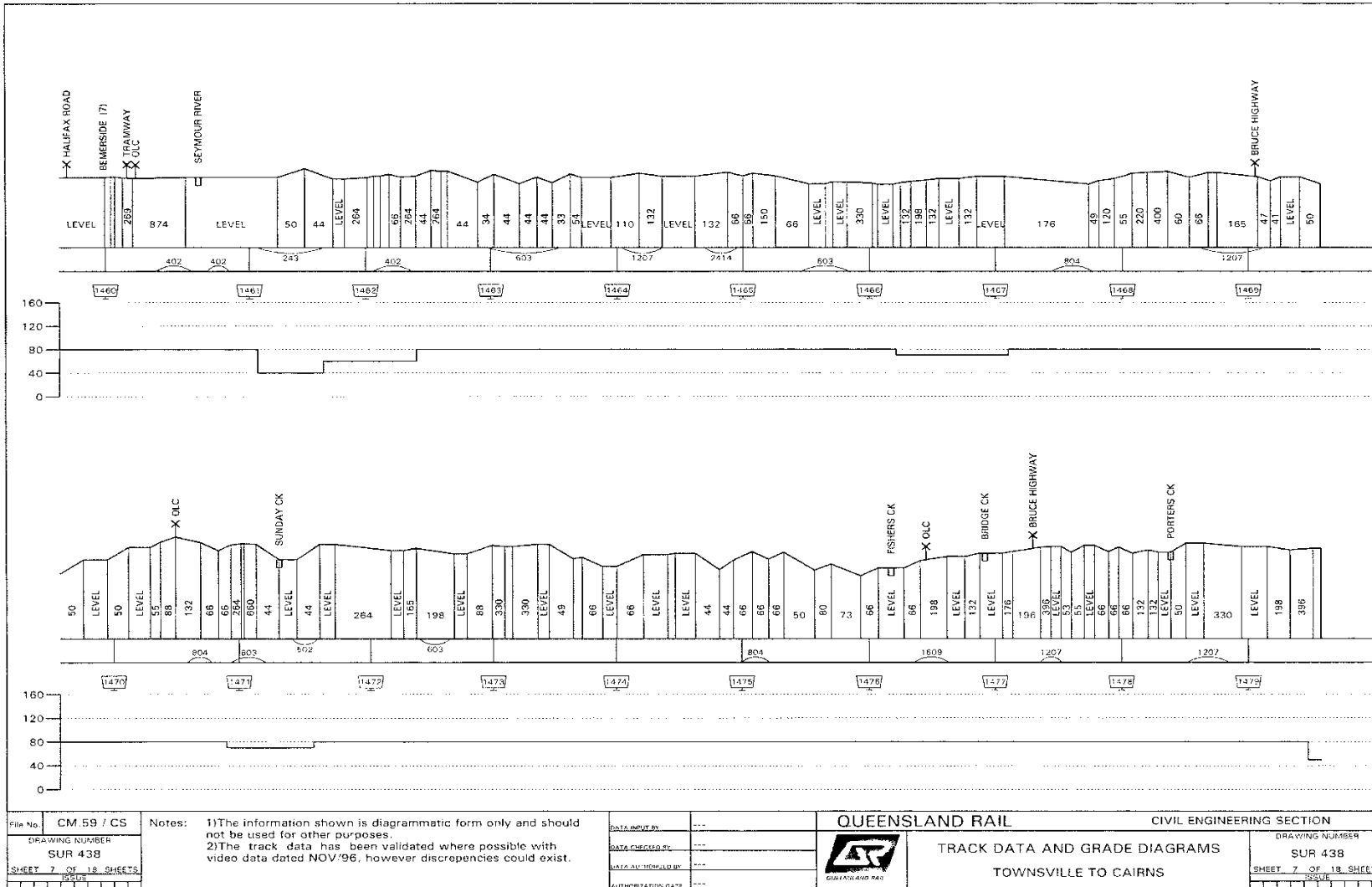
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File No.	CM 59 / CS
DRAWING NUMBER	SUR 438
SHEET	7 OF 18 SHEETS

Notes: 1)The information shown is diagrammatic form only and should not be used for other purposes.
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DATA CHECKED BY	---
DATA APPROVED BY	---
AUTHORIZATION DATE	---

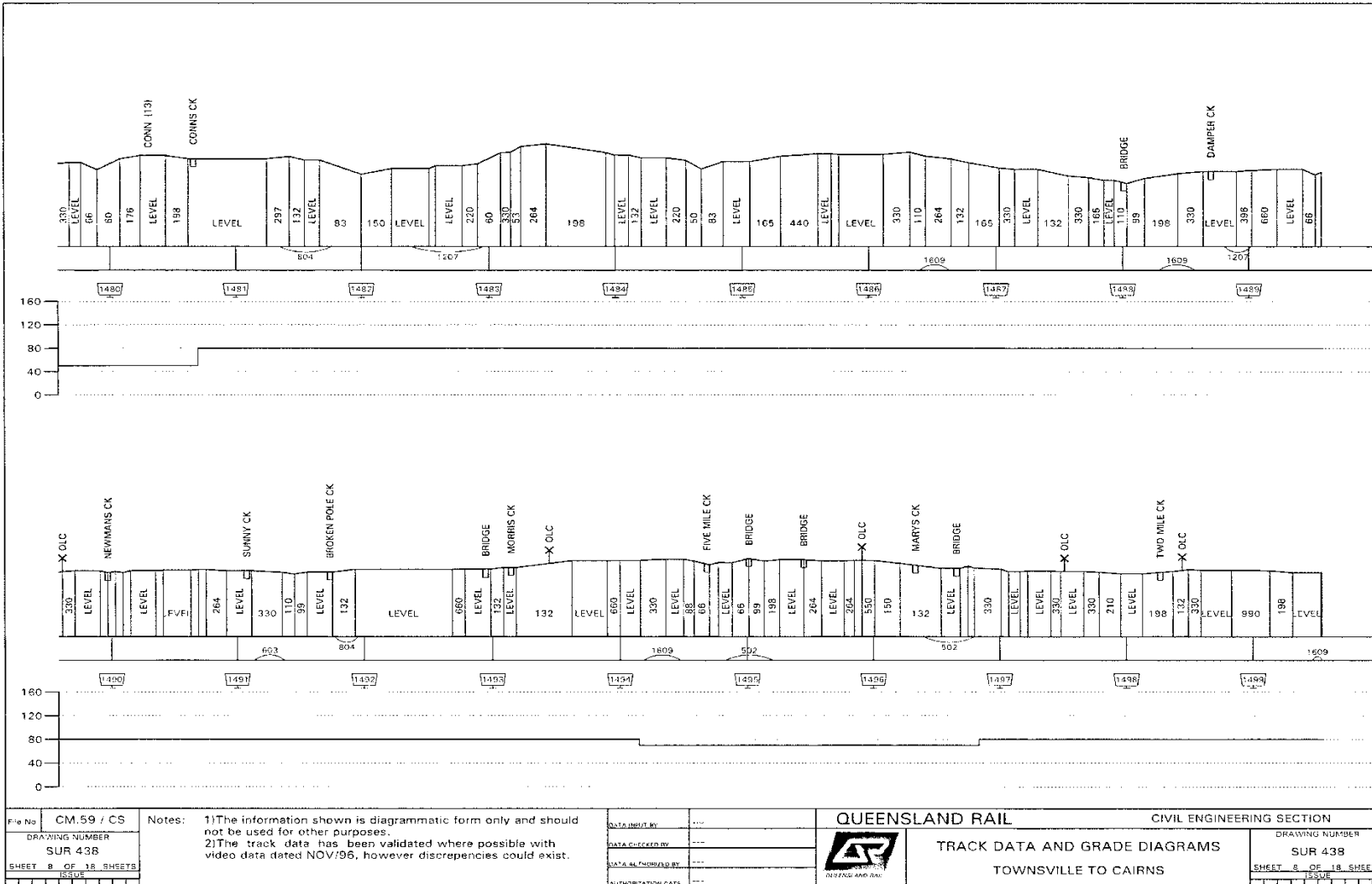
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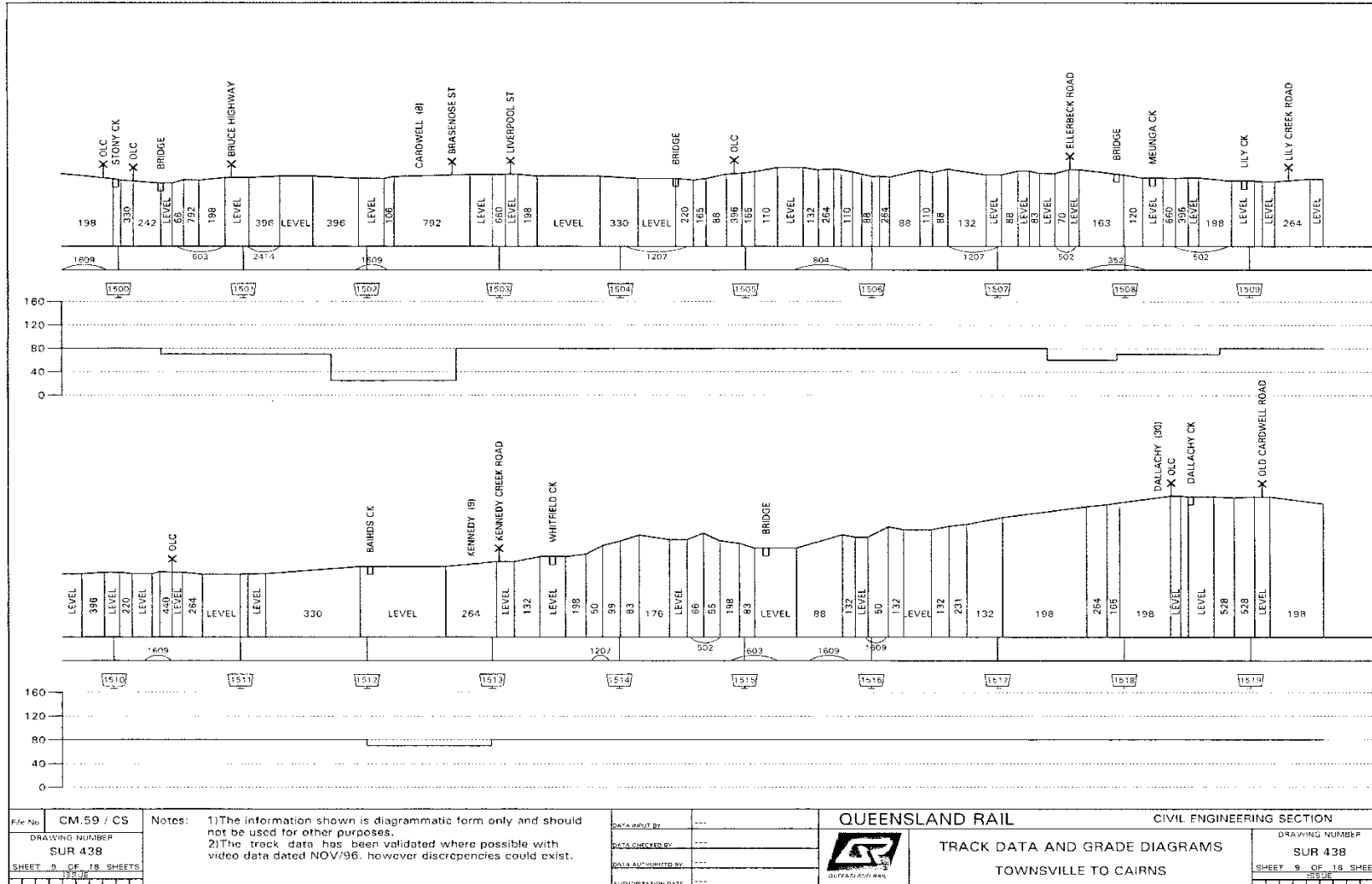
TRACK DATA AND GRADE DIAGRAMS
 TOWNVILLE TO CAIRNS

DRAWING NUMBER	SUR 438
SHEET	7 OF 18 SHEETS

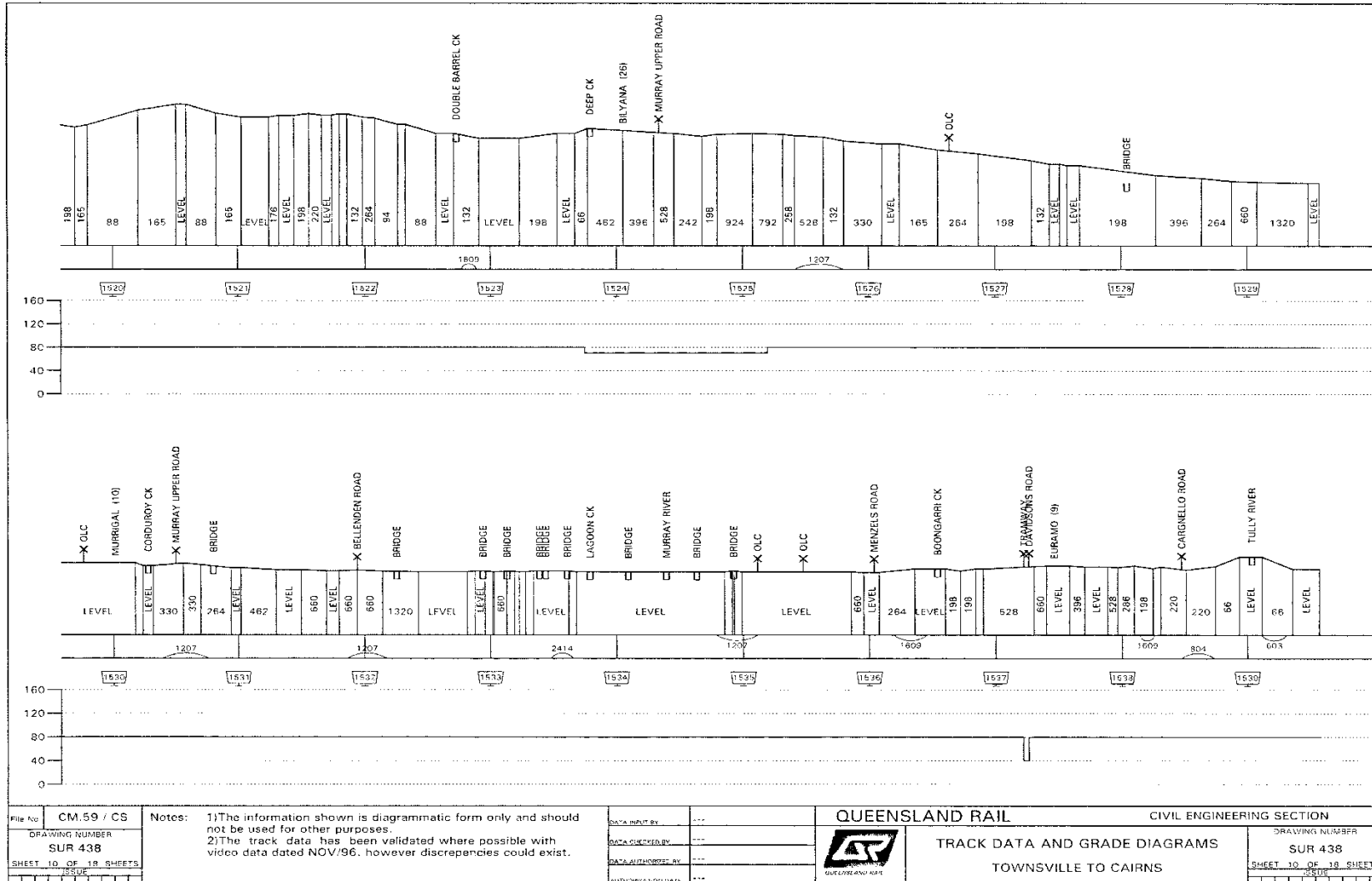
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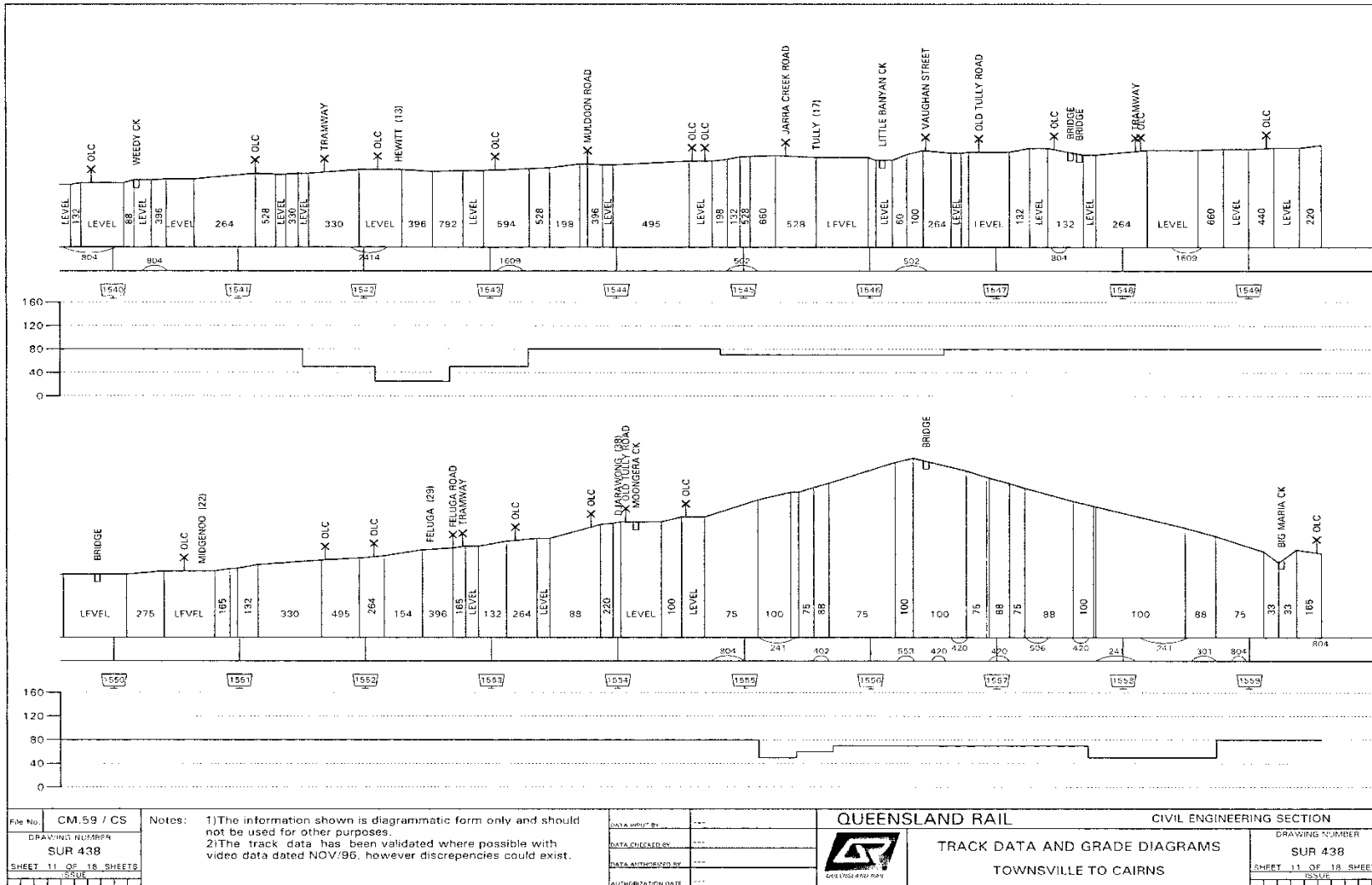
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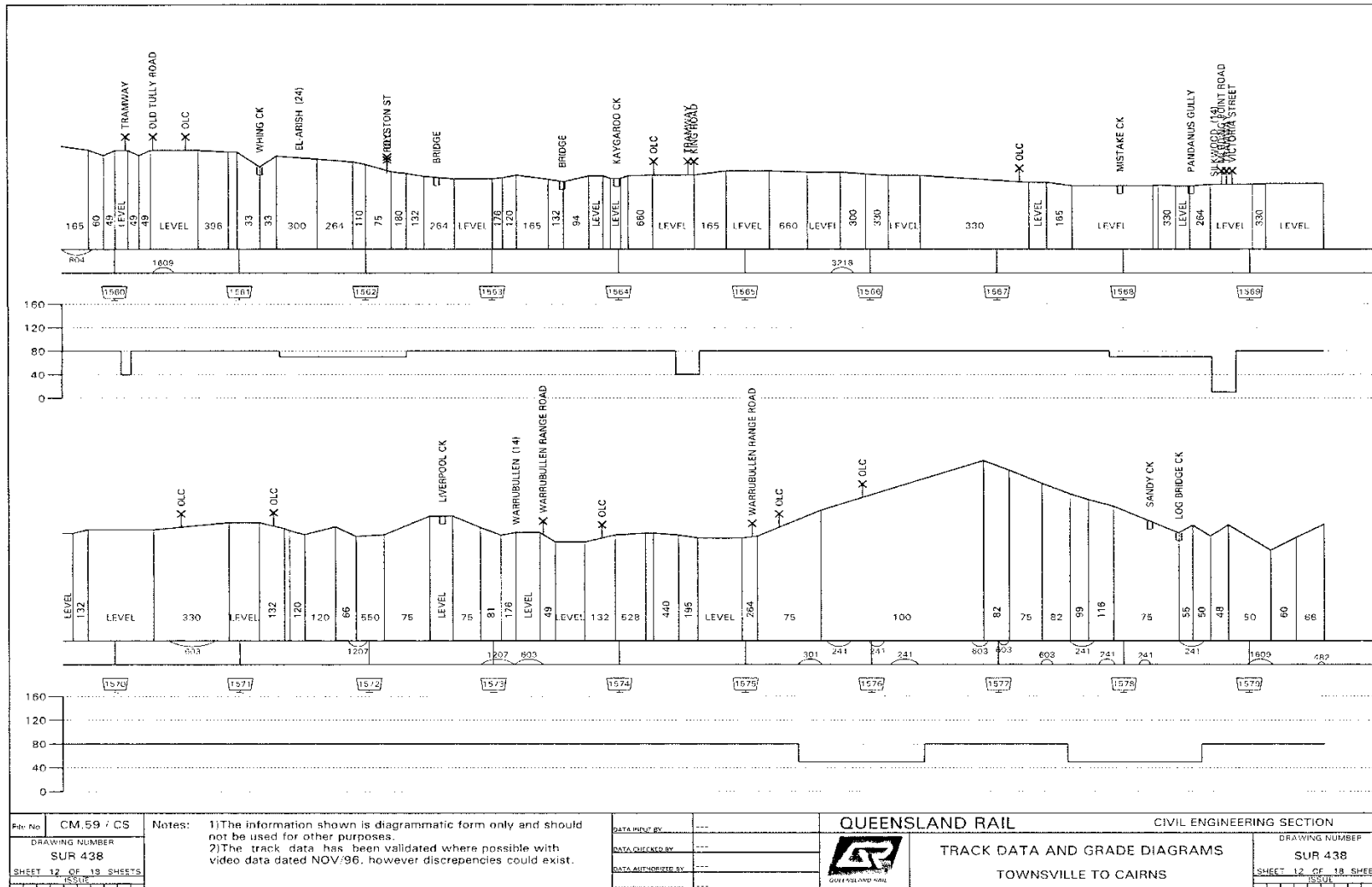
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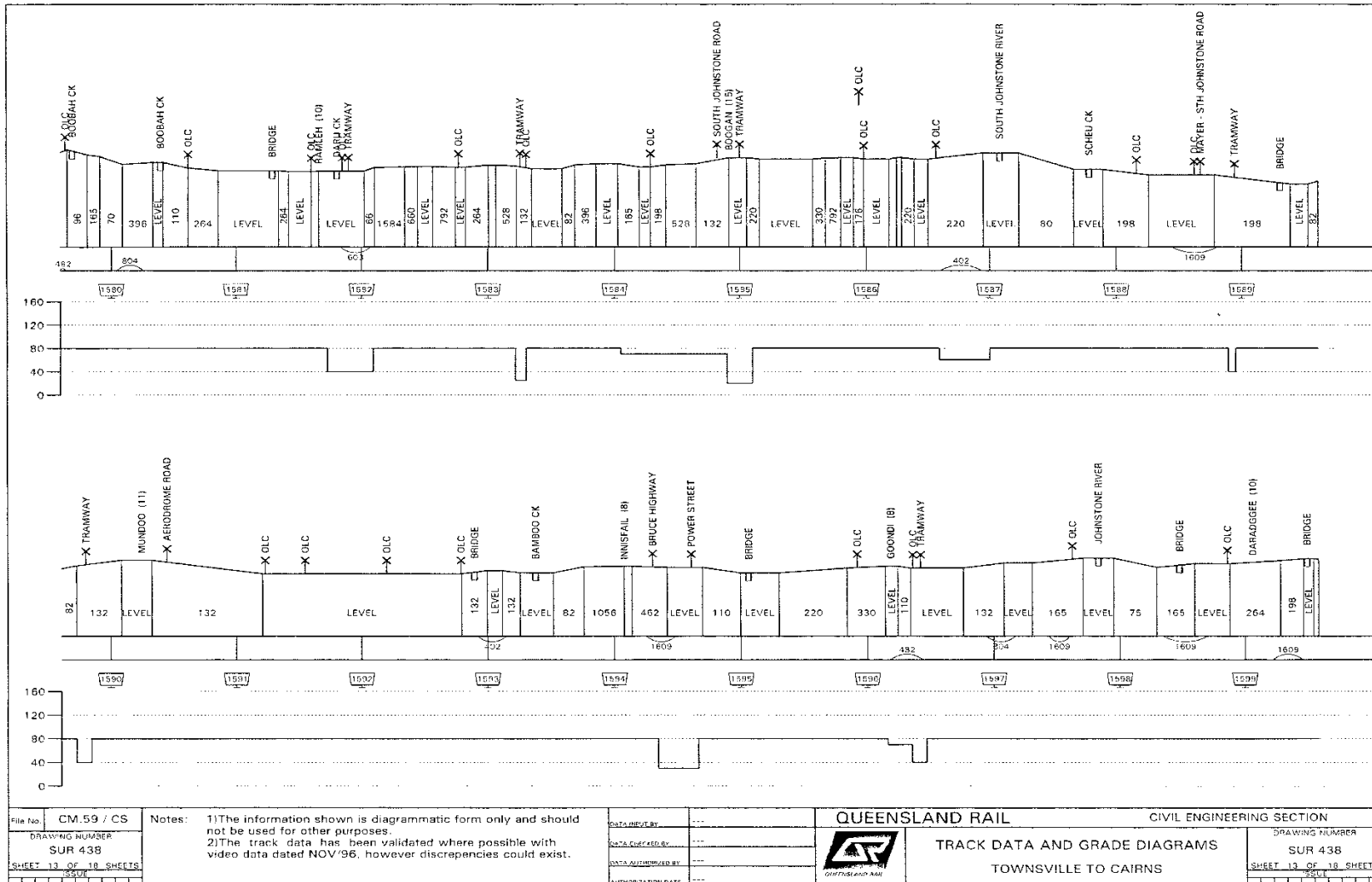
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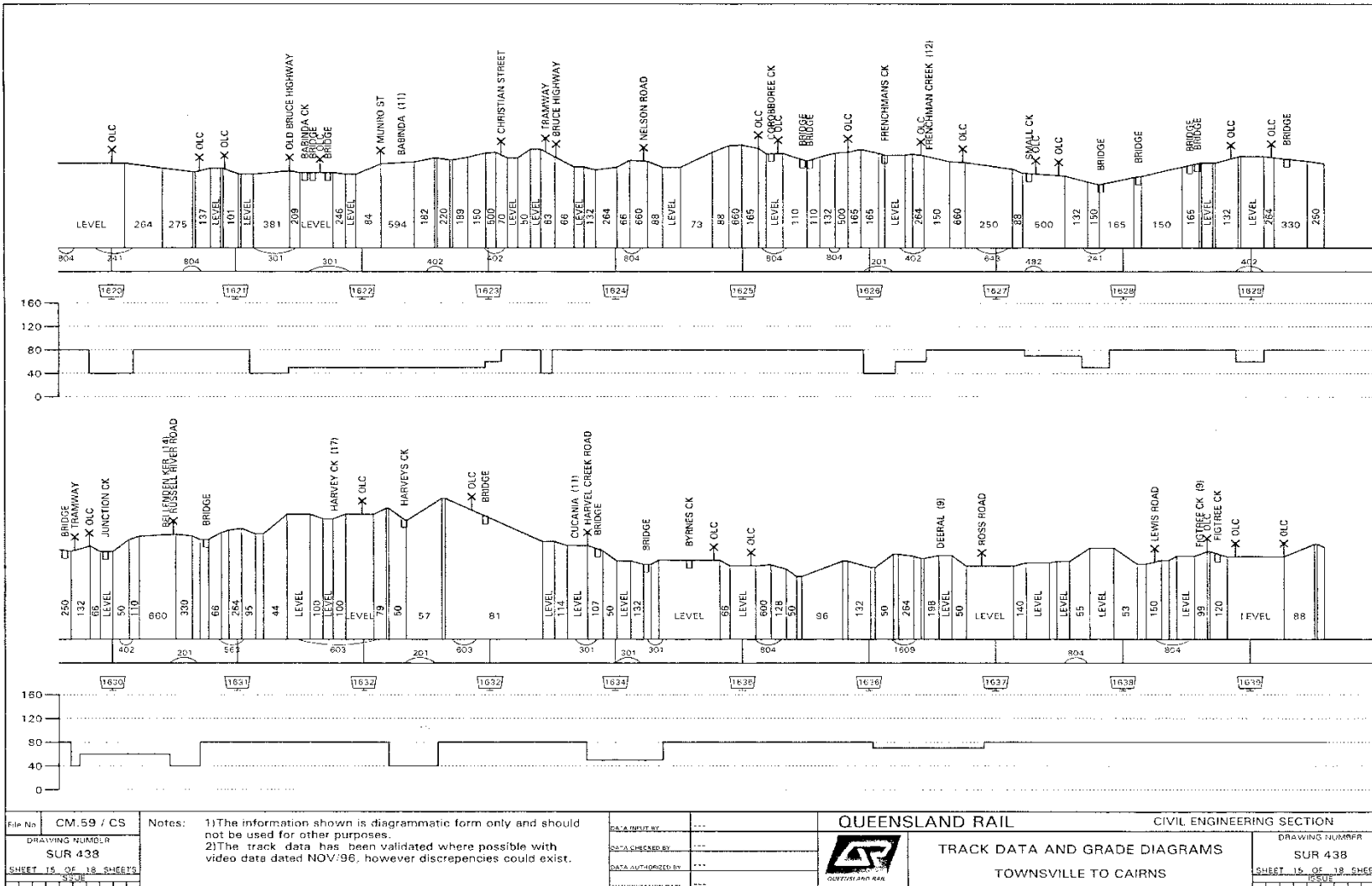


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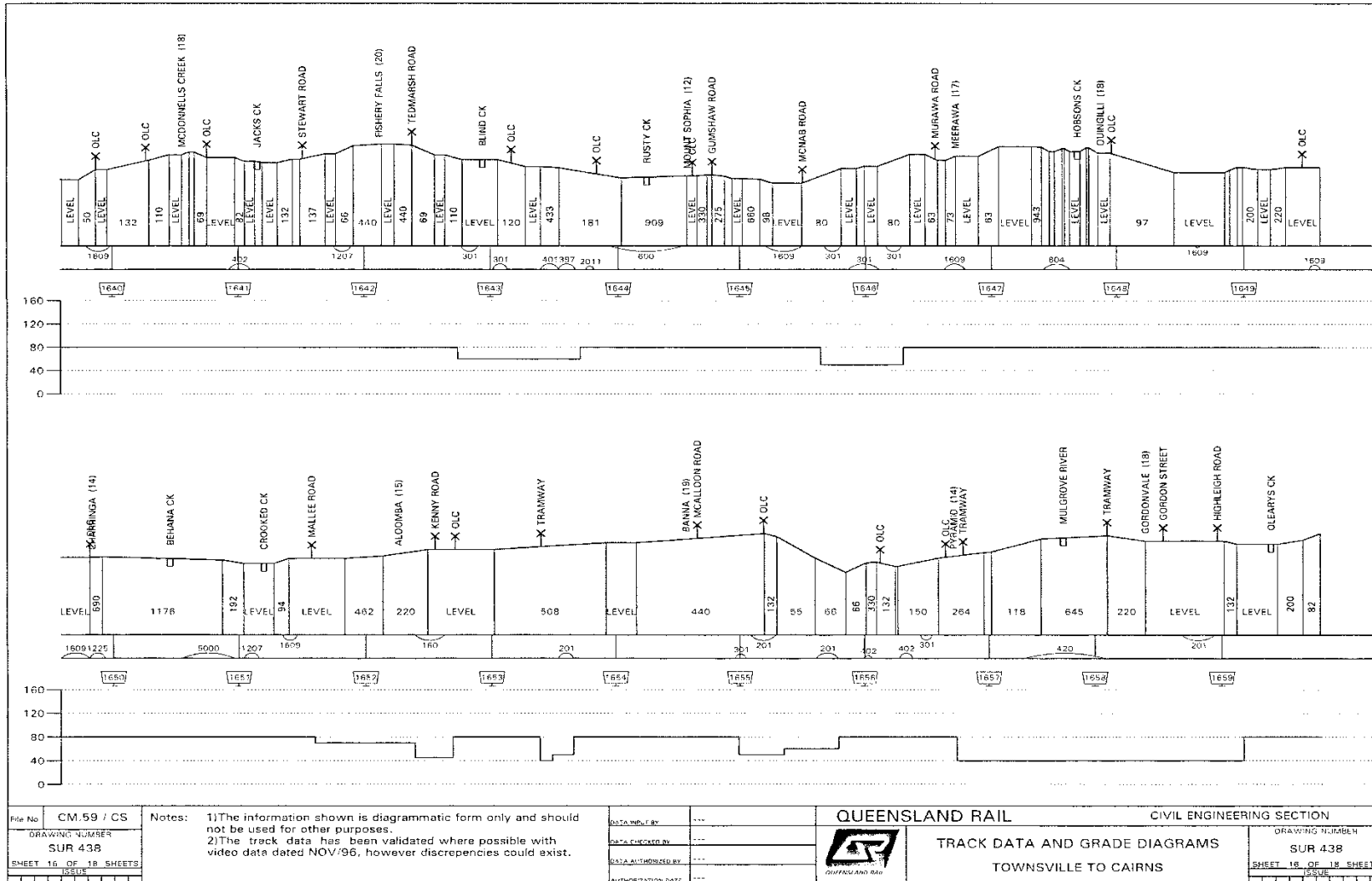


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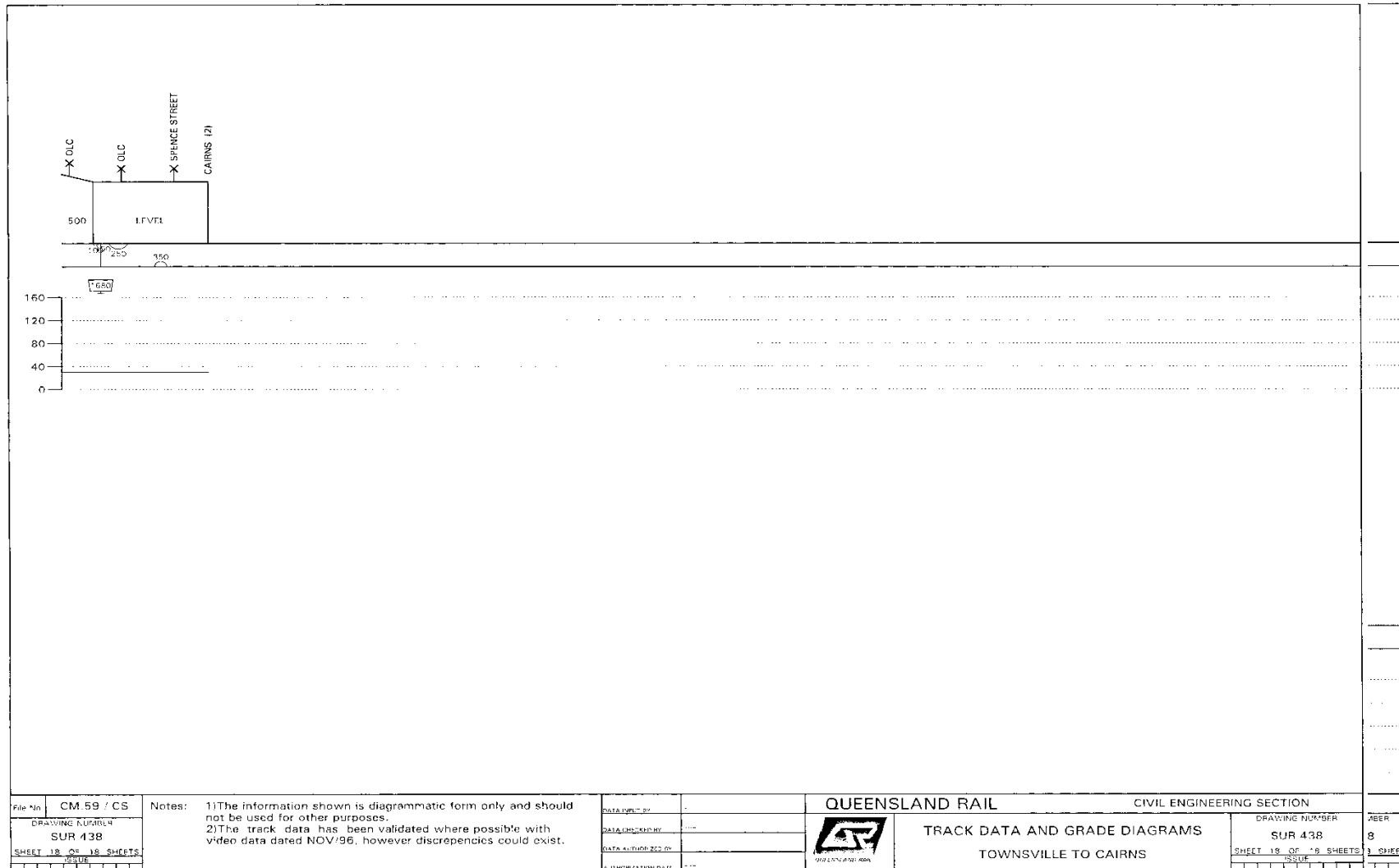
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APPENDIX F

Sectional Running Times

Location	Location	PASSENGER		80_FRT	
		UP	DOWN	UP	DOWN
		P/P	P/P	P/P	P/P
Rockhampton	Glenmore Junction	11	11	10	10
Glenmore Junction	Parkhurst	8	7	6	7
Parkhurst	The Caves	14	14	12	13
The Caves	Yaamba	10	10	11	9
Yaamba	Glen Geddes	14	15	16	15
Glen Geddes	Kunwarara	14	13	14	16
Kunwarara	Princhester	15	16	16	19
Princhester	Marlborough	11	11	10	13
Marlborough	Kooltandra	15	15	14	15
Kooltandra	Ogmore	13	12	14	13
Ogmore	Wumaiji	16	16	15	15
Wumaiji	St Lawrence	16	14	16	15
St Lawrence	Karlarka	18	18	15	16
Karlarka	Elalie	14	13	13	13
Elalie	Camila	14	9	13	14
Camila	Orkable	8	9	8	10
Orkable	Ilbilbie	12	13	13	13
Ilbilbie	Koumala	15	13	13	13
Koumala	Yukan	9	9	8	8
Yukan	Sarina	9	8	11	10
Sarina	Dawlish	7	8	9	10
Dawlish	Balberra	6	5	5	5
Balberra	Rosella	7	6	5	5
Rosella	Mackay	9	8	9	10
Mackay	Erakala	7	8	4	4
Erakala	Farleigh	4	4	5	6
Farleigh	Aminungo	12	10	10	12
Aminungo	Kuttabul	9	8	9	10
Kuttabul	Mount Ossa	10	9	11	9
Mount Ossa	Calen	10	10	10	9
Calen	Yalboroo	11	12	13	13
Yalboroo	Bloomsbury	14	15	16	15
Bloomsbury	Thoopara	14	13	15	14
Thoopara	Proserpine	13	13	14	14
Proserpine	Bubialo	12	15	17	17
Bubialo	Longford Creek	10	11	12	12
Longford Creek	Mookarra	12	13	12	13
Mookarra	Bowen Junction	10	8	10	9
Bowen Junction	Merinda	6	6	5	5
Merinda	Duroburra	2	2	2	2

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Location	Location	PASSENGER		80_FRT	
		UP	DOWN	UP	DOWN
		P/P	P/P	P/P	P/P
Durroburra	Kall	6	8	6	7
Kall	Wathana	3	3	3	3
Wathana	Wilmington	8	7	8	9
Wilmington	Guthlungra	11	12	13	13
Guthlungra	Gumlu	12	11	11	12
Gumlu	Bobawaba	9	9	10	10
Bobawaba	Inkerman	8	9	9	9
Inkerman	Home Hill	10	10	9	9
Home Hill	Ayr	13	14	8	9
Ayr	Pioneer	9	11	9	9
Pioneer	Baratta	11	9	9	8
Baratta	Giru	9	5	8	7
Giru	Cromarty	5	6	7	6
Cromarty	Storth	6	5	7	6
Storth	Nome	12	11	12	10
Nome	Julago	7	6	4	4
Julago	Sunmetal Junction	1	1	1	1
Sunmetal Junction	Partington	3	2	2	2
Partington	Stuart	2	3	3	3
Stuart	Cluden	2	3	3	3
Cluden	Oonoonba	6	6	2	2
Oonoonba	Townsville Fork Points	3	3	2	1
Townsville Fork Points	Townsville New Station	3	3	4	4
Townsville New Station	Garbutt	7	7	13	13
Garbutt	Nightjar	9	10	7	8
Nightjar	Deeragun	1	1	3	3
Deeragun	Cobarra New Leg	7	6	6	7
Cobarra New Leg	Cobarra Old Leg	1	1	1	1
Cobarra Old Leg	Purono	4	5	3	3
Purono	Kurukan	9	9	12	12
Kurukan	Rollingstone	12	12	13	13
Rollingstone	Mutamee	12	12	12	11
Mutamee	Bambaroo	11	12	11	11
Bambaroo	Pombel	10	10	10	11
Pombel	Ingham	12	12	12	12
Ingham	Hinchinbrook	13	13	19	20
Hinchinbrook	Conn	15	14	14	14
Conn	Cardwell	20	18	18	18
Cardwell	Kennedy	10	11	11	12
Kennedy	Bilyana	11	12	10	10

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		PASSENGER		80_FRT	
		UP	DOWN	UP	DOWN
Location	Location	PIP	PIP	PIP	PIP
Bilyana	Hewitt	16	16	18	17
Hewitt	Tully	5	4	5	5
Tully	El Arish	20	20	17	18
El Arish	Silkwood	10	9	8	8
Silkwood	Boogan	19	20	17	17
Boogan	Mundoo	5	6	8	9
Mundoo	Innisfail	7	6	5	6
Innisfail	Waugh	17	20	17	18
Waugh	Babinda	16	16	17	17
Babinda	Deeral	16	18	16	16
Deeral	Alomba	14	15	14	13
Alomba	Gordonvale	7	7	7	7
Gordonvale	Kamma	7	8	7	8
Kamma	Woree QRX SDG	9	9	9	9
Woree QRX SDG	Portsmith	5	6	5	5
Portsmith	Calms	7	7		
Mackay	Walkerston			10	10
Walkerston	Marian			18	18
Erakala	Mackay Harbour			10	15
Home Hill	Carstairs			15	20
Ayr	Kalamia Mill			20	20
Pioneer	Pioneer Sugar Mill			20	15
Giru	Invicta (Giru) Sugar Mill			10	10
Julago	Stuart Meatworks SDG			5	5
Stuart	Stuart Yard	5	5	5	5
Stuart Yard	Cluden	6	6	6	7
Townsville Fork Points	Townsville			5	5
Townsville	Townsville Jetty			5	5
Townsville New Station	Townsville			2	2

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APPENDIX G

Altitudes

Location	Altitude in metres
Rockhampton	8
Glenmore Junction	11
Kawana	14
Olive	19
Parkhurst	31
The Caves	59
Yaamba	19
Glen Geddes	78
Kunwarara	48
Princhester	79
Marlborough	87
Kooltandra	50
Ogmore	19
Styx	15
Wumalgi	1
St Lawrence	12
Kalarka	11
Clairview	9
Elalie	16
Carmila	17
Orkobie	22
Ilbilbie	40
Koumala	30
Yukan	17
Sarina	18
Dawlish	12
Balberra	15
Rosella	11
Mackay	9
Erakala	11
Farleigh	40
Aminungo	39
Kuttabul	38
Mount Ossa	11
Kolijo	20
Calen	23
Wagoora	37
Yalboroo	55
Bloomsbury	46
Thoopara	34
Gunyarra	15
Proserpine	11
Bubialo	27
Longford Creek	15
Mookarra	12
Bowen	3

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Merinda	8
Euri	12
Wathana	20
Guthalungra	13
Gumlu	14
Bobawaba	8
Inkerman	7
Home Hill	11
Ayr	10
Pioneer	5
Barratta	5
Giru	4
Cromarty	4
Storth	6
Nome	19
Stuart	18
Townsville	3
Garbutt	5
Deeragun	11
Yabulu	16
Purono	11
Kurukan	12
Rollingstone	22
Mutarnee	15
Bambaroo	15
Pombel	13
Ingham	12
Bemerside	7
Conn	13
Cardwell	7
Kennedy	9
Bilyana	26
Tully	16
El Arish	23
Silkwood	13
Boogan	15
Innisfail	8
Waugh	14
Babinda	11
Deeral	9
Aloomba	16
Gordonvale	19
Edmonton	17
Cairns	3
Ooralea	12
Walkerston	21

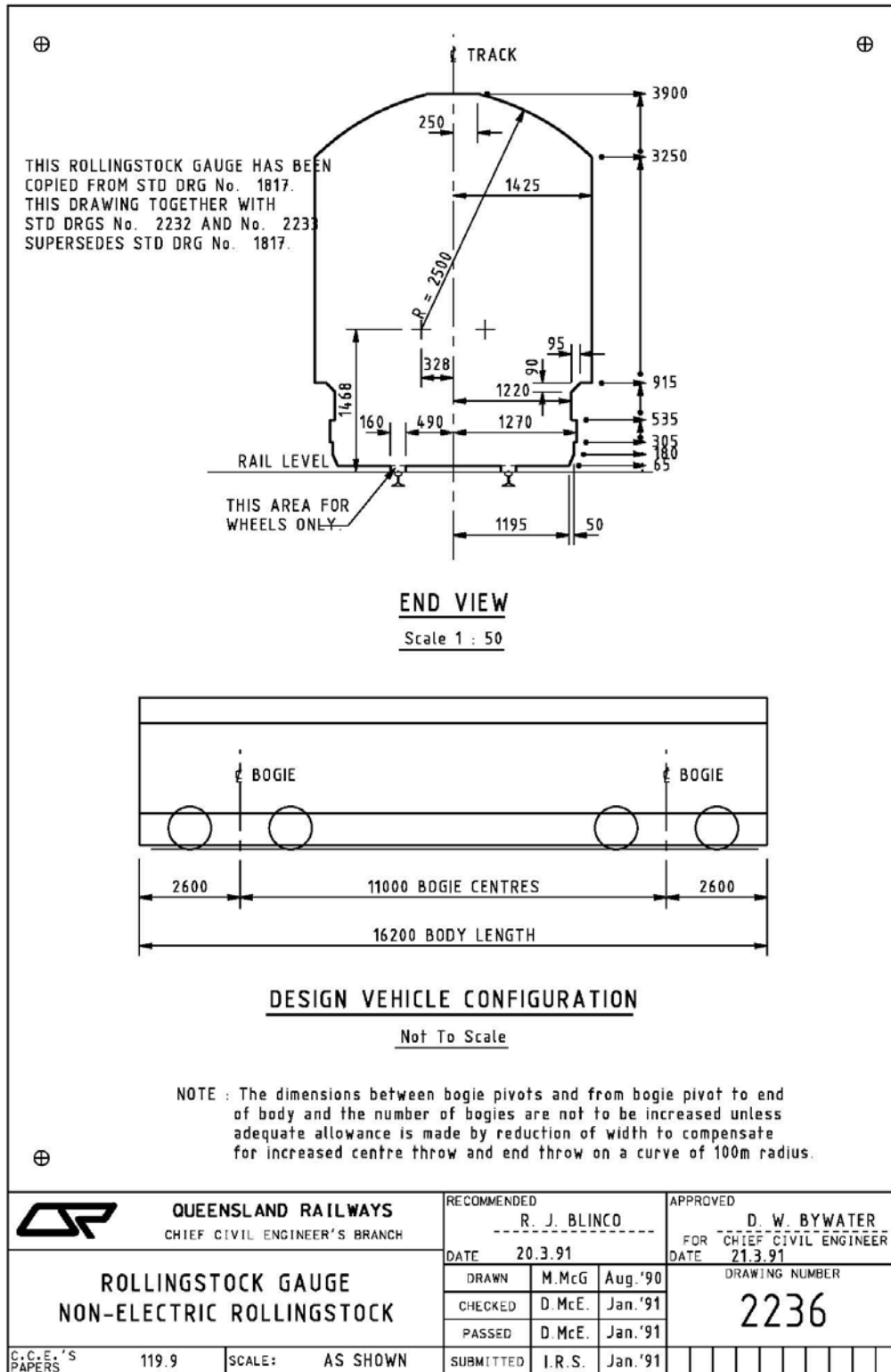
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Marian	42
North Rockhampton	7
Lakes Creek	7
Nerimbera	6
Fields Siding	8
Sleipner	27
Mount Chalmers	73
Bondoola	56
Yeppoon	10

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APPENDIX H

Rollingstock Gauges



APPENDIX I

Trackside Detection Equipment

Weathering Monitoring Stations are installed on the North Coast Line at the following locations :-

Orkobie	872.040 km	Rainfall / Flood /
Temperature Dawlish	936.350 km	F / T
Yalboroo	1028.100 km	R / F / T
Proserpine	1081.440 km	R / F / T
Guthalungra	1199.930 km	R / F / T
Bambaroo	1424.230 km	T / R
Conn	1480.300 km	T / R
Five Mile Creek	1494.710 km	T / F / R
Bilyana (Corduoy Ck)	1530.290 km	T / F / R
Murray River	1534.410 km	F
Babinda Creek	1621.510 km	F / T
Frenchman's Creek	1626.480 km	F
Cod Fish Creek	1627.270 km	F
Junction Creek	1629.950 km	F
Harvey's Creek	1632.310 km	F
Deeral (Fishery Falls)	1642.110 km	T / R

These monitoring stations currently use the MSR to send an SMS text message to Network Control and the local Manager Infrastructure Maintenance alerting them to a potential problem that needs to be monitored / managed.